

**ORDINANCE NO. 2022-248**

**AMENDING ZONING ORDINANCE – CHAPTER 114**

**WHEREAS**, on December 19, 2018 and following an extensive public involvement process, the Dothan City Planning Commission adopted the *Highway 84 East Master Plan* (Resolution 2018-1) and did recommend its ratification to the Dothan City Commission; and,

**WHEREAS**, on January 15, 2019, the Dothan City Commission ratified the Highway 84 East Master Plan (Resolution 2019-4); and,

**WHEREAS**, on July 6, 2021, the Dothan City Commission entered into a contract with DesignWorkshop to develop an ordinance to implement the vision of the Highway 84 East Master Plan (Resolution 2021-175); and,

**WHEREAS**, DesignWorkshop organized and conducted two community workshops and held individual meetings with property owners and stakeholders in the study area to gather input and develop overlay district regulations to implement the vision of the Highway 84 East Master Plan; and,

**WHEREAS**, after a public hearing held on July 20, 2022, the Planning Commission recommended the proposed amendment of the Zoning Ordinance to the Dothan City Commission for consideration; and,

**WHEREAS**, in accordance with Section 11-52-77(2) of the Code of Ala. 1975, notice of the proposed amendment ran for three consecutive weeks in the legal section and one time in the regular section of the *Dothan Eagle* as a display ad; and,

**WHEREAS**, Article X will be reformatted to Divisions to accommodate the addition of the text of the US Highway 84 East Overlay District.

**NOW, THEREFORE, BE IT ORDAINED** by the Board of Commissioners of the City of Dothan, Alabama, as follows;

**Section 1.** That Article X, Special District be reformatted into Divisions creating Division 1. Generally, to include Section 114-155 amended as follows:

Special Districts are created and shall be known as a Planned Unit Development (PUD) District, the US Highway 84 East Overlay District and Manufactured Home Community (MHC) District. Division 2 contains the regulations for establishing a Planned Unit Development (PUD). A PUD may be established in any district provided the use(s) intended to be established are permitted in the underlying district; depending on location, rezoning may be required. Division 3 contains regulations that apply in a fixed geographic area to be known as the US Highway 84 East Overlay District. Uses in the US Highway 84 East Overlay District are specifically defined as either Permitted or Special Exceptions uses. Division 4 contains regulations that apply to the establishment of a Manufactured Home Community (MHC) and to individual manufactured homes parked on private lots. An MHC may be established only in an MH-1 district.

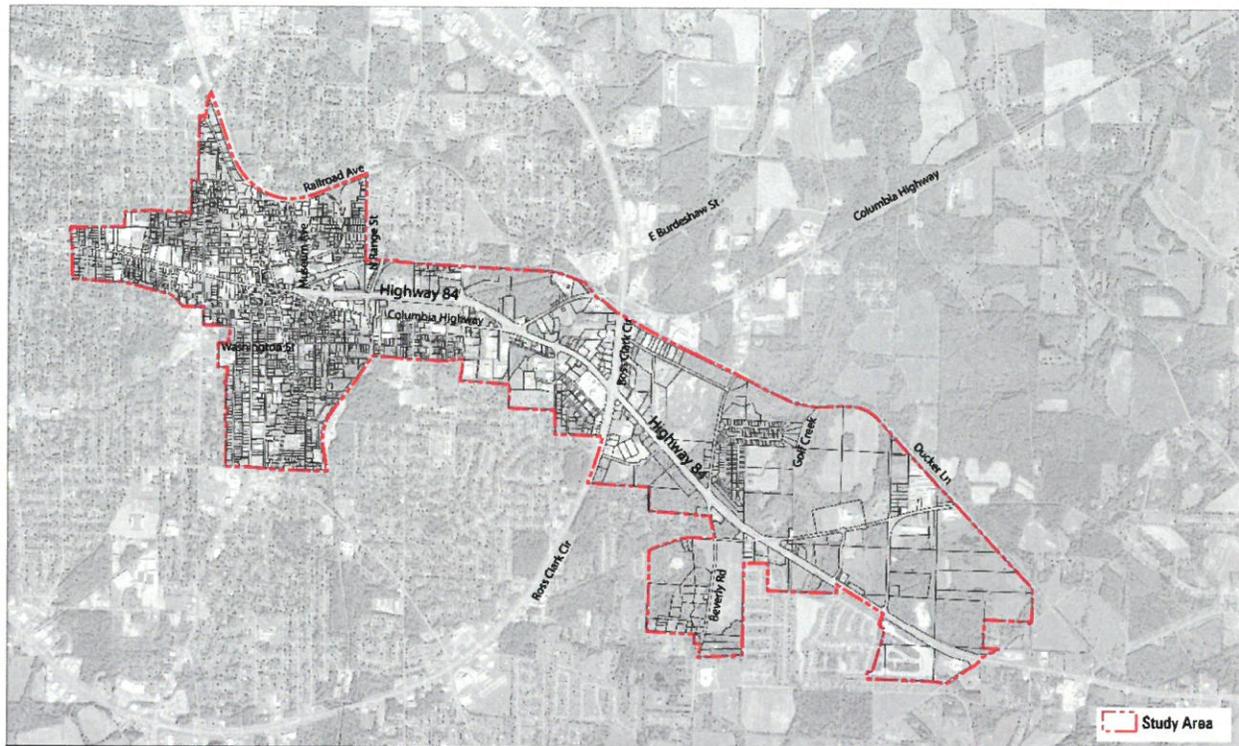
**Section 2.** That Article X, Special Districts, Section 114-157 be deleted in its entirety and replaced with the following:

DIVISION 3. US 84 EAST OVERLAY DISTRICT

**US Highway 84 East Overlay District**

**Section 114-161. Generally**

- (a) Purpose. The purpose of this section is to implement the vision of the Highway 84 East Vision Plan by establishing clear development and use standards consistent with the Plan in an Overlay District focused on design quality, flexibility of implementation and incentives. The Highway 84 East Vision promotes incremental transformation of the area from automobile-oriented development patterns to pedestrian-oriented development patterns. The Highway 84 East Vision also promotes greater diversity of land uses and building typologies resulting in districts containing daily needs near each other including places to live, to work, to recreate, to eat, to shop and to socialize. This Overlay shall result in development that is more coordinated, despite fragmented ownership and multiple builders, and to achieve public benefits as specified in the Highway 84 East Vision, including:
- (1) Corridor development resulting in high quality streetscapes, new buildings and adaptive reuse of historic structures displaying heightened architectural integrity.
  - (2) Establish higher density, mixed-use downtown districts that smoothly transition to lower density districts.
  - (3) Development of new streets and new blocks to accommodate mixed-use development and alternative vehicular routes to Highway 84. New streets will include sidewalks and streetscapes to promote walking and biking, and a high-quality sense of place.
  - (4) Prioritize how new buildings interface with sidewalks, streets, and other buildings to create desirable spaces. Specifically focus on the fronts of the buildings and how buildings address the sidewalk and shape the public spaces.
  - (5) Prioritize the design and form of buildings and de-emphasize the specific use of the buildings to support market adaptation.
  - (6) Prioritize building massing and materials and de-emphasize specific architectural style.
  - (7) Promote design improvements without slowing economic development, especially for small, difficult to redevelop parcels fronting on Highway 84.
  - (8) Streamline approvals for projects that comply with the development standards and intent of the Highway 84 East Vision Plan.
- (b) Applicability.
- (1) Applicability of Zoning Overlay. The Highway 84 East Overlay District is established to serve as an overlay to the existing base zone districts. Base zone districts within the Overlay include AC, B-1, B-2, B-3, H-I, L-I, O-I, R-1, R-2, R-3, R-4, and R-A. To the extent that anything in this Chapter conflicts with a base zone district, this Chapter shall prevail.
  - (2) Applicability of Historic District Design Guidelines. Commercial and residential design guidelines adopted by the Historic Preservation Commission and apply within the Downtown Historic District and NBCAR Historic District.
  - (3) Overlay Boundaries.
    - a. The Overlay District is hereby established in accordance with Map 1.

**Map 1 – Overlay Boundary**

**HIGHWAY 84 OVERLAY DISTRICT  
STUDY AREA MAP**

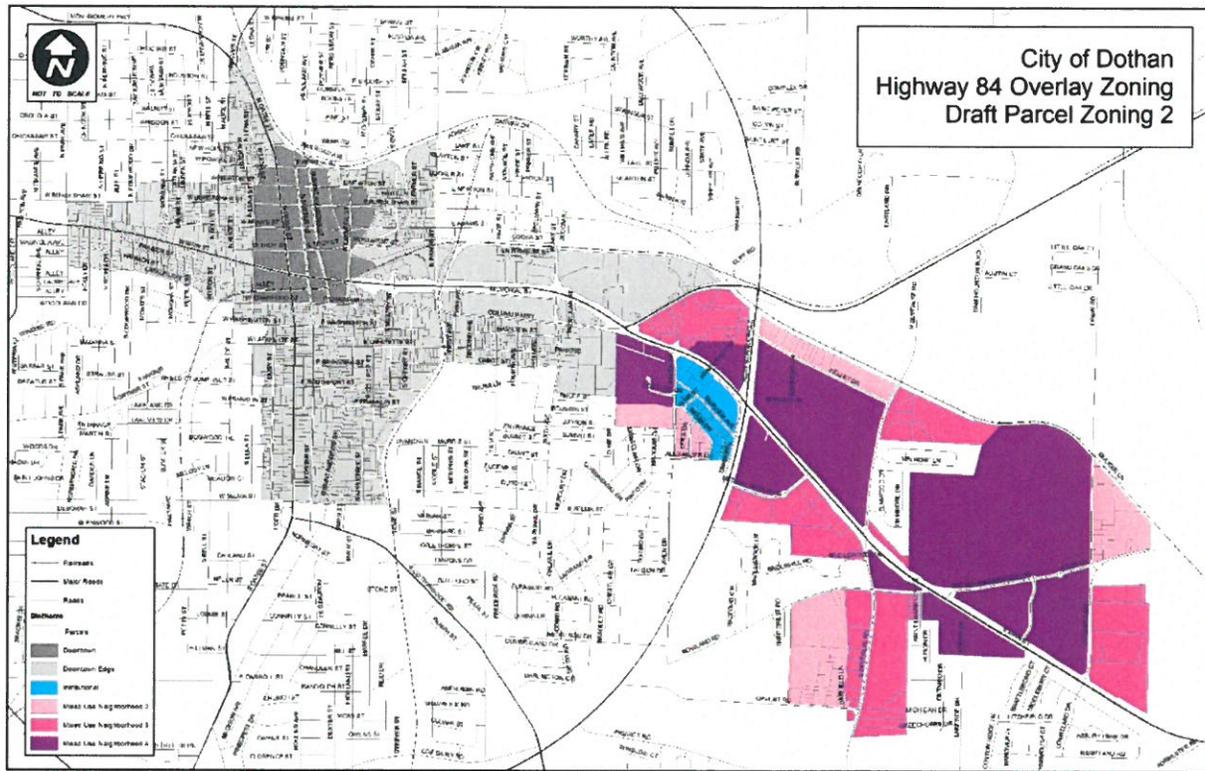


- (4) Applicability for Development. The Overlay shall apply to all development within its boundaries. When demolition of a building or structure is triggered, all new or replacement development shall comply with the requirements of the Overlay. When an addition is proposed, the addition shall comply with the Overlay. When a remodel is proposed, it shall comply with the Overlay to the greatest extent possible.
- (5) Subdistrict Boundaries. Within the Highway 84 East Overlay District, six (6) Subdistricts are established to provide clear transitions throughout the corridor and to ensure appropriate land use transitions and that development standards are appropriate for their context. The Subdistricts are illustrated in Map 2 and include the following:
  - a. Downtown Core.
  - b. Downtown Edge.
  - c. Mixed-Use 4.
  - d. Mixed-Use 3.
  - e. Mixed-Use 2.
  - f. Institutional.
- (6) Alternative Building Placement Standards. The Highway 84 East Overlay District provides alternatives in the placement of buildings for the Mixed-Use 4, Mixed-Use 3, Mixed-Use 2, and Institutional Subdistricts. The preferred alternative promotes building placement towards the front of the property and parking placed behind the building. This is consistent with the Highway 84 East Vision Plan by emphasizing buildings and their architecture rather than parking lots. However, for development plans that elect to place parking toward the front of the building, specific requirements are defined for the design of front parking lots that improve aesthetics, promote green stormwater infrastructure,

and provide abundant landscaping. Optional Building Placement standards are not allowed within the Downtown Core and Downtown Edge Subdistricts.

The two (2) building placement alternatives are defined as the “Preferred” alternative and the “Optional” alternative. These alternatives apply only to four (4) Subdistricts: Mixed-Use 4, Mixed-Use 3, Mixed-Use 2, and Institutional. Specific development standards, dimensional standards and parking standards for each alternative are detailed in section “D” of the ordinance.

## Map 2 – Subdistricts



### (c) General Requirements.

#### (1) Text Controls.

- a. In the event of a conflict or inconsistency between the text of this Chapter and any illustration, figure, table, heading, caption, or map, the text shall control.
- b. Unless otherwise specifically indicated, lists or items or examples that use terms such as “for example,” “including,” and “such as,” or similar language are indented to provide examples and are not an exhaustive list of all possibilities.

(2) All development applications requiring a building permit shall be in conformance with this Chapter.

(3) All land area within a project site, including outdoor use of space, off-street parking, interior drives and other circulation ways, community facilities such as schools, recreation centers, libraries, shopping, and public safety facilities, may be counted in computing Lot Area.

- (4) Any deviations from the Overlay District requirements will require the approval of the City Planning Commission.

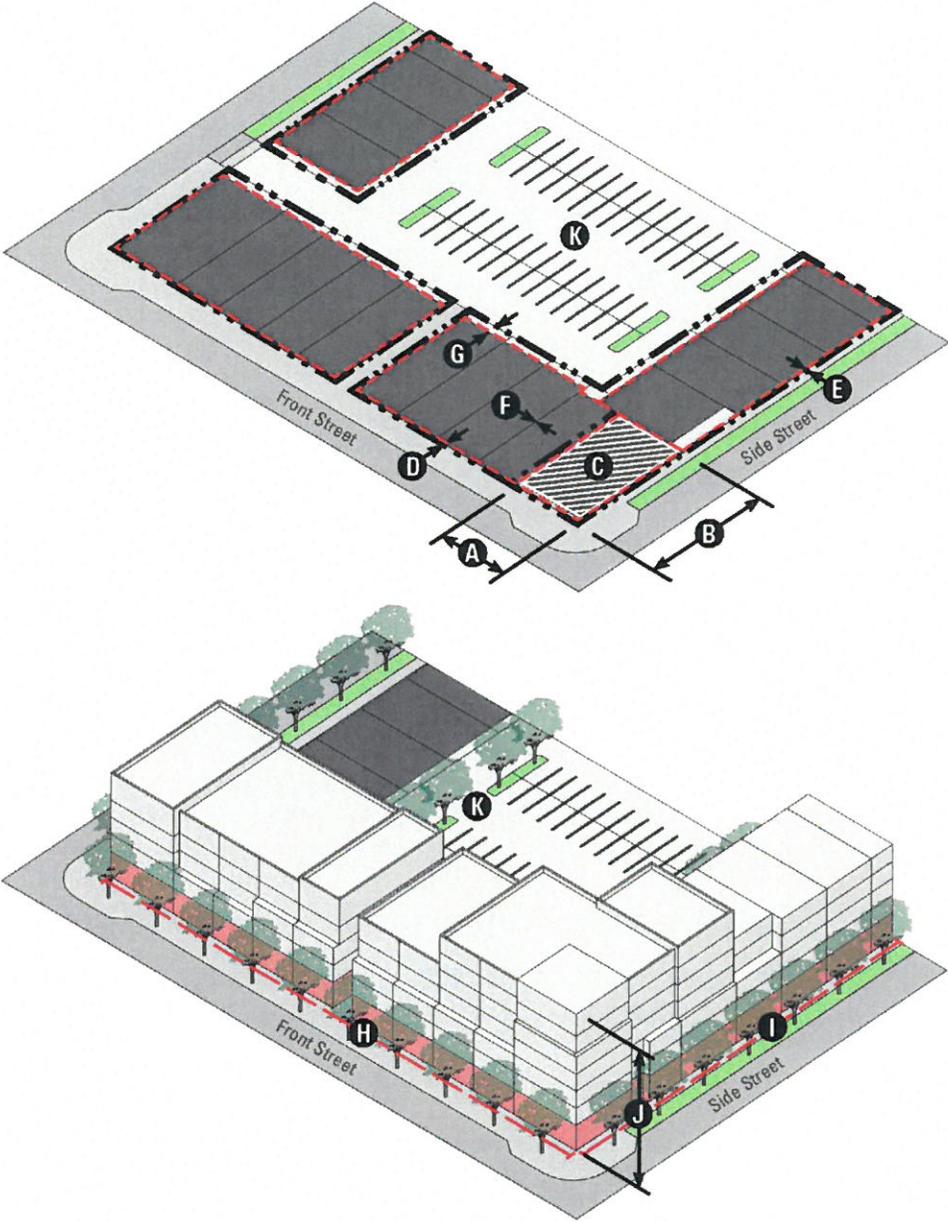
**Section 114-162. Subdistrict Development Standards**

- (a) Downtown Core Subdistrict. The purpose of the Downtown Core Subdistrict is to provide a high-density, vibrant, urban downtown that provides locally- and regionally serving commercial, entertainment, and civic and public uses, as well as a variety of urban housing choices in main street mixed-use, mid-rise, and high-rise building types. The standards allow for redevelopment that is consistent with the historic urban design pattern of downtown. The following Dimensional Tables (Tables 1-1, 1-2, and 1-3) apply to the Downtown Core Subdistrict. Graphics highlighting the dimensions follow in Figure 1.

Table 1-1 Downtown Core Dimensional Standards Table					
Lot Size	Lot Coverage*		Building Placement		Building Height
Minimum Width (A) 15 feet	Maximum (C)	100%	Front Setback (D)	0 feet	Main Building (J) 7 floors max. 4 Floors max facing Foster Street.
Minimum Depth (B) 50 feet	Minimum	n/a	Side Street (E)	0 feet	
			Side Interior (F)	0 feet	
			Rear (G)	0 feet	
			Building Façade at Build-to-Line:		
			Front (H)	0 feet	
				80% min.	
			Side Street (I)	0 feet	
				60% min.	

\* Lot coverage includes both buildings and hardscape areas including parking lots, walkways, sidewalks, patios, etc.

**Figure 1: Downtown Core Development Standards Illustrations**



- A** Minimum Width: 15 feet
- B** Minimum Depth: 50 feet
- C** Maximum Lot Coverage: 100%  
Minimum Lot Coverage: n/a
- D** Front Setback: 0 feet
- E** Side Street Setback: 0 feet
- F** Side Interior Setback: 0 feet
- G** Rear Setback: 0 feet
- H** Building Façade at Build-to-Line:  
Front: 0 feet, 80% min.
- I** Building Façade at Build-to-Line:  
Side Street: 0 feet, 60% min.
- J** Building Height - Main Building:  
7 floors max.
- K** Parking Lot Location

**Table 1-2**  
**Downtown Core Parking Requirement Table**

**Parking Requirements**

*Downtown Parking is provided in shared public parking lots and on streets. Minimal parking is required to encourage a walkable downtown and to limit the impacts of large surface parking lots. The number of required parking spaces do not need to be provided on site and can utilize shared parking lots off site of the property being developed.*

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements. Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

Land Use	Vehicular Parking*				Bicycle Parking
	Minimum	Maximum	Reductions Available	Off-Site Parking	Minimum
Residential*	No minimums	N/A	N/A	N/A	0.5 spaces per unit
Non-Residential					
< 5,000 sq. ft. of Useable Space	No minimums	N/A	N/A	Up to 1,500 ft. ADA-accessible walking distance	Depending on use per Section 114-182 Sub-Section (F)
> 5,000 sq. ft. of Useable Space	No minimums	N/A	See Below		
On-street parking and off-site may be allowed to count towards requirement at the discretion of the Director of Planning & Development.					

Parking Lot Location		Figure 1 Reference
Proximity to main building	Behind main building	K
When no building is provided		
Front	30 feet from Property Line	
Street Side	10 feet from Property Line	

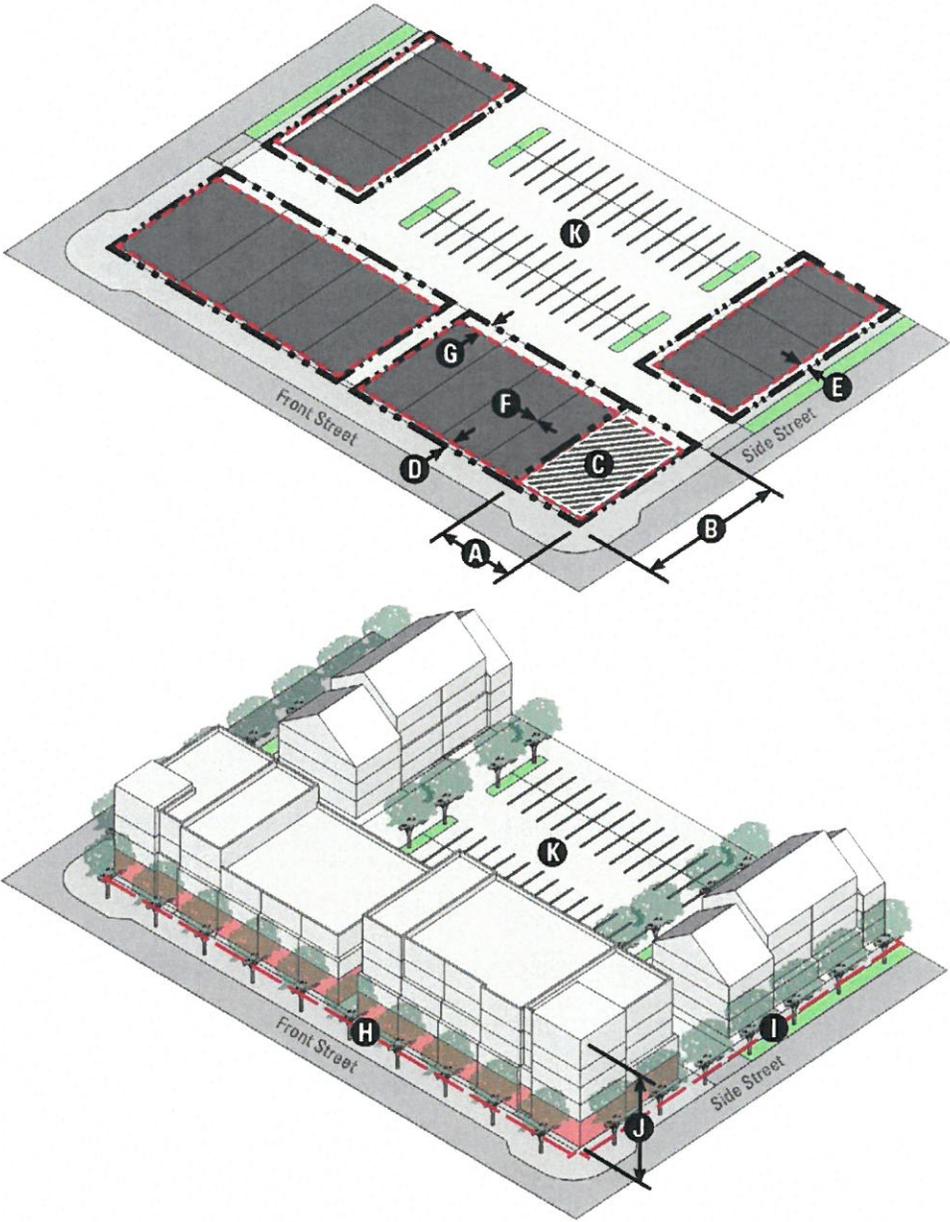
**STANDARD PARKING REDUCTIONS**

N/A

- (b) Downtown Edge Subdistrict. The purpose of the Downtown Edge Subdistrict is to provide a high density, vibrant, urban development that is supportive of the Downtown Core. The Downtown Edge Subdistrict serves commercial, entertainment, civic and public uses. This district also serves a variety of urban housing choices in main street mixed-use, and mid-rise types. The Downtown Edge is located adjacent to the Downtown Core and is a transition area to surrounding, lower density areas. The following Dimensional Tables (Tables 2-1, 2-2, and 2-3) apply to the Downtown Edge Subdistrict. Graphics highlighting the dimensions follow in Figure 2.

<b>Table 2-1</b>				
<b>Downtown Edge Dimensional Standards Table</b>				
<b>Lot Size</b>	<b>Lot Coverage</b>		<b>Building Placement</b>	<b>Building Height</b>
Minimum Width (A) 20 feet min.	Maximum (C)	80%	Front Setback (D) 5 feet	Main Building (J) 5 floors max.
Minimum Depth (B) 50 feet min.	Minimum	n/a	Side Street (E) 5 feet Side Interior (F) 0 feet for attached development  5 feet for all other development types  Rear (G) 15 feet  Building Façade at Build-to-Line: Front (H) 5 feet 60% min. Side Street (I) 5 feet 50% min.	

**Figure 2: Downtown Edge Development Standards Illustrations**



- A** Minimum Width: 20 feet min.
- B** Minimum Depth: 50 feet min.
- C** Maximum Lot Coverage: 80%  
Minimum Lot Coverage: n/a
- D** Front Setback: 5 feet
- E** Side Street Setback: 5 feet
- F** Side Interior Setback: 5 feet
- G** Rear Setback: 15 feet
- H** Building Façade at Build-to-Line:  
Front: 5 feet, 60% min.
- I** Building Façade at Build-to-Line:  
Side Street: 5 feet, 50% min.
- J** Building Height - Main Building:  
5 floors max.
- K** Parking Lot Location

**Table 2-2  
Downtown Edge Parking Requirement Table**

*Downtown Edge Parking is provided in shared parking lots and on streets. Minimal parking is required to encourage a walkable downtown and to limit the impacts of large surface parking lots. The number of required parking spaces do not need to be provided on site and can utilize shared parking lots off site of the property being developed.*

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements. Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

Land Use	Vehicular Parking*				Bicycle Parking
	Minimum	Maximum	Reductions Available or Alternative Requirements	Off-Site Parking	Minimum
Residential					
Single-Family Homes	1.5 spaces per unit	150% of minimum	See below	N/A	No requirement
Townhomes	1.0 spaces per unit				
Multi-Family	No minimums			Up to 1,000 ft ADA-accessible walking path	0.5 space per unit
Hotel/Motel	No minimums	N/A	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 4 rooms
Restaurant					
Fast Food**	No minimums	N/A	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 1,000 sq. ft.
All Other	No minimums				
Office					
Medical	No minimums	N/A	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft.
All Other	No minimums				
General Retail & Service	No minimums	N/A	See below	Up to 1,000 ft. ADA-accessible walking path	1 space per 2,000 sq. ft.

\* On-street and off-site parking may be allowed to count towards requirement at the discretion of the Director of Planning & Development.

\*\* At least four queuing spaces are required for drive-throughs.

Parking Lot Location		Figure 2 Reference
Proximity to main building	Behind main building	K
When no building is provided		
Front	30 feet from Property line	
Street Side	10 feet from Property line	

**STANDARD PARKING REDUCTIONS**

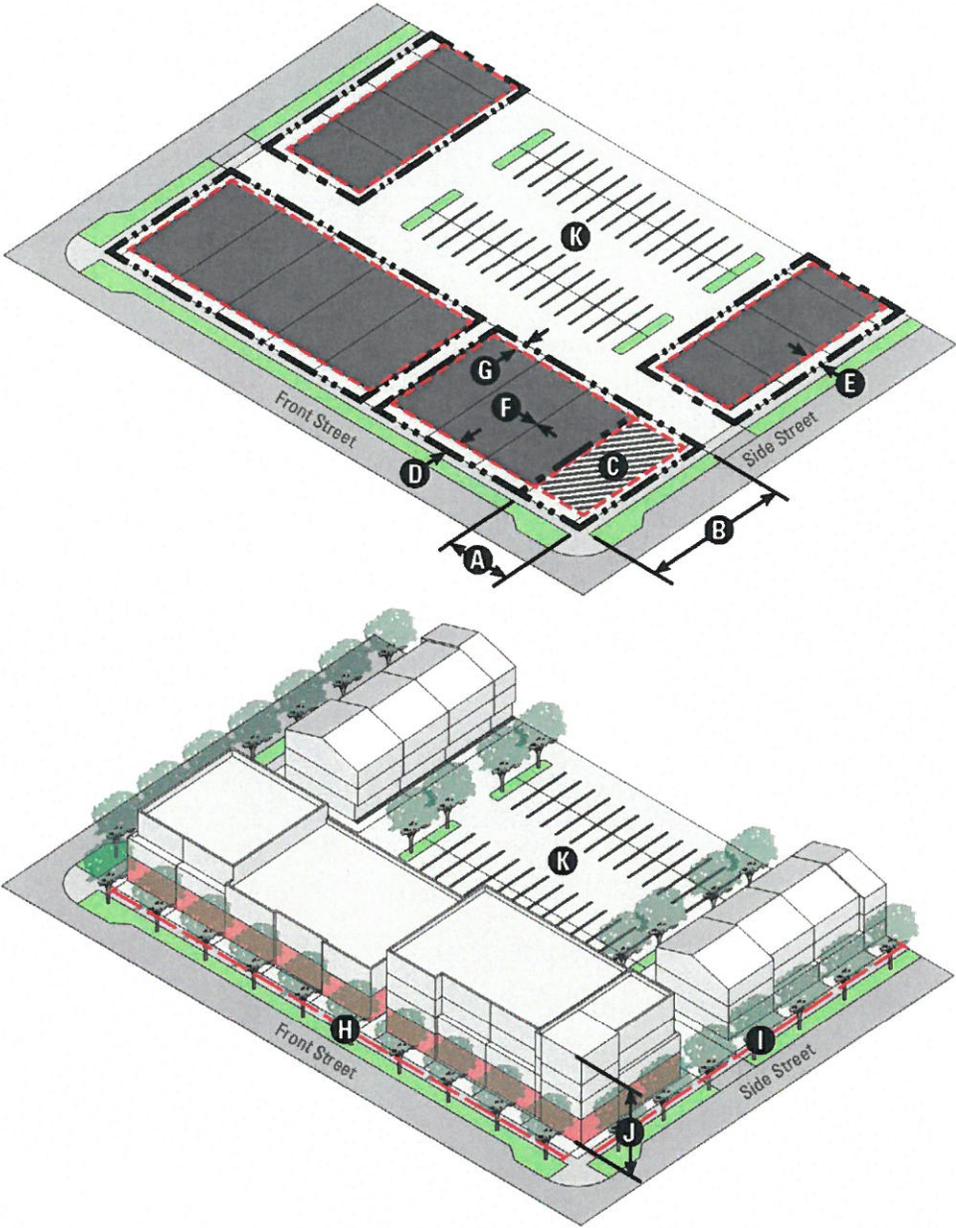
Standard reductions are available for developments that provide certain amenities or that implement certain strategies that provide alternatives to single-occupancy vehicles. Standard reductions can be granted administratively and deducted automatically from the applicable minimum parking requirement if the stated criteria are met, as determined by the Director of Planning and Development These amenities and strategies are identified in Table 2-3 below.

Table 2-3 Downtown Edge Standard Parking Reductions	
Amenity/Strategy	Standard Reduction
Park or plaza of at least 5% of the site area, open to the public at least 8 hours per day.	15% reduction

- (c) **Mixed-Use 4 Subdistrict.** The purpose of the Mixed-Use 4 Subdistrict is to provide a vibrant, walkable mixed-use area that provides commercial, retail, entertainment, and civic and public uses. This district also serves a variety of housing choices. While less dense than the Downtown Subdistricts, these areas encourage the same mixture of uses found in the Core and Edge. The following Dimensional Tables (Tables 3-1, 3-2, and 3-3) apply to the Mixed-Use 4 Subdistrict. Graphics highlighting the dimensions follow in Figure 3.

<b>Table 3-1</b>			
<b>Mixed-Use 4 Dimensional Standards Table</b>			
<b>Lot Size</b>	<b>Lot Coverage</b>	<b>Building Placement</b>	<b>Building Height</b>
Minimum Width (A)	Maximum (C)	Front Setback (D)	Main Building (J) 4 floors max.
20 feet min.	70%	8 feet for all streets except Highway 84 and Ross Clark Circle.	
		20 feet facing Highway 84 and Ross Clark Circle	
Minimum Depth (B)	Minimum	Side Street Setback (E) 5 feet	
80 feet min.	n/a	20 feet facing Highway 84 and Ross Clark Circle	
		Side Interior Setback (F) 0 feet	
		Rear Setback (G) 5 feet	
		Building Façade at Build-to-Line*: Front (H) 8 feet - 60% of building face minimum.	
		Side Street (I) 5 feet - 50% min.	
		*Build-to-line is required for all street facing buildings except for buildings facing Highway 84 and Ross Clark Circle.	

**Figure 3: Mixed-Use 4 “Preferred” Development Standards Illustrations**

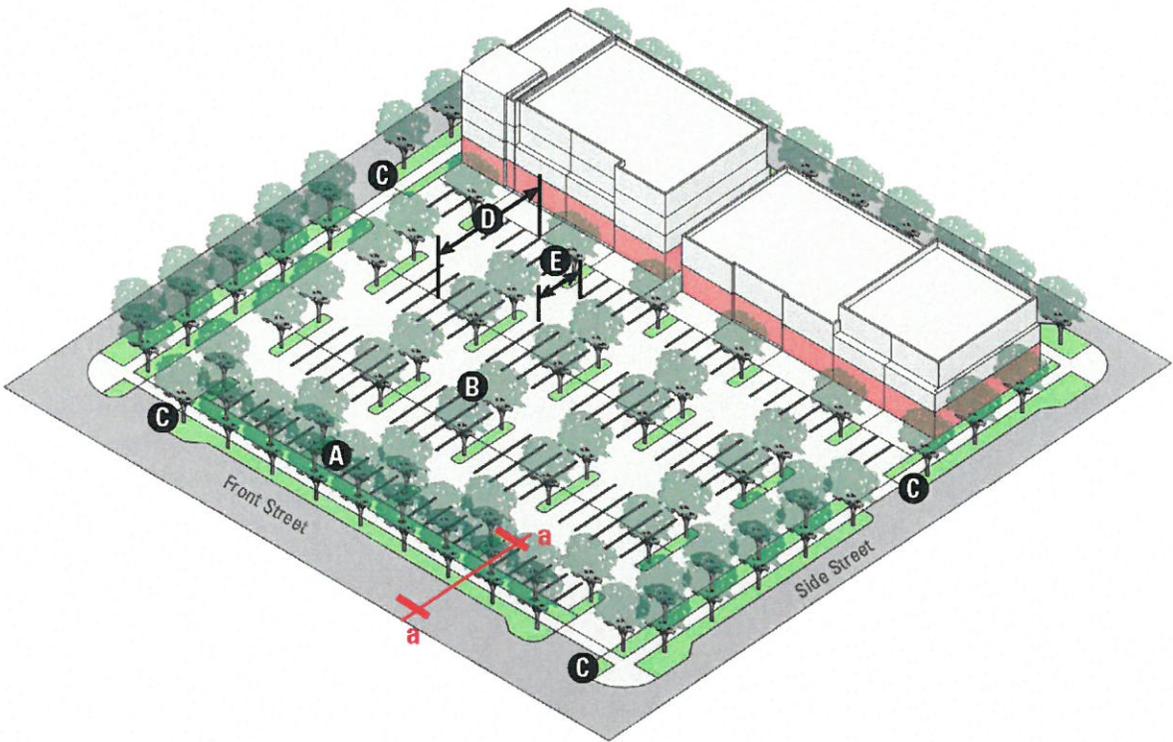


- A** Minimum Width: 20 feet min.
- B** Minimum Depth: 80 feet min.
- C** Maximum Lot Coverage: 70%  
Minimum Lot Coverage: n/a
- D** Front Setback: 8 feet

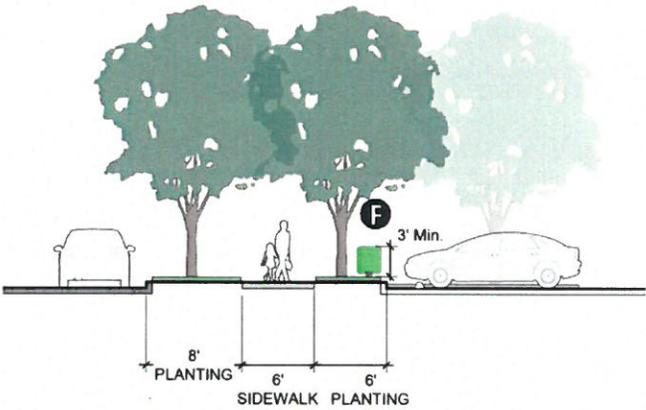
- E** Side Street Setback: 5 feet
- F** Side Interior Setback: 0 feet
- G** Rear Setback: 5 feet
- H** Building Façade at Build-to-Line:  
Front: 8 feet, 60% min.

- I** Building Façade at Build-to-Line:  
Side Street: 5 feet, 50% min.
- J** Building Height - Main Building:  
4 floors max.
- K** Parking Lot Location

**Figure 3: Mixed-Use 4 “Optional” Development Standards Illustrations**



- A** Continued street trees, maximum 30 feet apart
- B** Planter island with canopy trees every 6 stalls
- C** Parking lot entry/ exit
- D** Minimum 60 feet
- E** Minimum 24 feet
- F** Minimum 3 feet tall planting or wall



**SECTION a-a**

<b>Table 3-2 Mixed-Use 4 Parking Requirement Table</b>					
<b>Parking Requirements</b>					
<b>Land Use</b>	<b>Vehicular Parking</b>				<b>Bicycle Parking</b>
	<b>Minimum</b>	<b>Maximum</b>	<b>Reductions Available or Alternative Requirements</b>	<b>Off-Site Parking</b>	<b>Minimum</b>
<b>Residential*</b>					
Single-Family Homes	1.5 spaces per unit	150% of minimum	See below	N/A	150% of minimum
Townhomes	1.0 spaces per unit				
Multi-Family	Studio/Efficiency: 0.7 spaces/unit 1-Bedroom: 1.0 space/unit 2-Bedroom and above: 1.25 spaces/unit			N/A	0.5 space per unit
Hotel/Motel	0.75 spaces/room plus 1 space per 1,000 sq. ft. meeting space	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 4 rooms
<b>Restaurant</b>					
Fast Food**	1 space per 100 sq. ft. of seating area	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 1,000 sq. ft.
All Other	1 space per 200 sq. ft. of seating area				
<b>Office</b>					
Medical	3 spaces per 1,000 sq. ft.	150% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft.
All Other	2 spaces per 1,000 sq. ft.				
General Retail & Service	2 spaces per 1,000 sq. ft.	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft.

\* On-street parking may be allowed to count towards requirement at the discretion of the Director of Planning & Development.

\*\* At least 2 queuing spaces are also required for drive-throughs.

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements. Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

<b>Parking Lot Location</b>		<b>Figure 3 Reference</b>
Proximity to main building	Unless parcels are less than 2.0 acres of size at time of Overlay adoption 75% of provided parking spaces are required to be located behind main building. A small amount (no more than 25%) of convenience parking can be located between Highway 84 proposed buildings.	K
When no building is provided	Large retail centers with anchor / big box stores greater than 45,000 SF are exempt from this requirement.	
Front	10 feet from Property Line	
Street Side	10 feet from Property Line	

**ALTERNATIVE MINIMUM PARKING REQUIREMENTS**

Use	Alternative Requirement
Independent Senior Living (55+)	0.75 spaces per unit
Assisted Senior Living (55+)	0.25 spaces per unit

**STANDARD PARKING REDUCTIONS**

Standard reductions are available for developments that provide certain amenities or that implement certain strategies that provide alternatives to single-occupancy vehicles. Standard reductions can be granted administratively and deducted automatically from the applicable minimum parking requirement if the stated criteria are met, as determined by the Director of Planning and Development. These amenities and strategies are identified in Table 3-3 below.

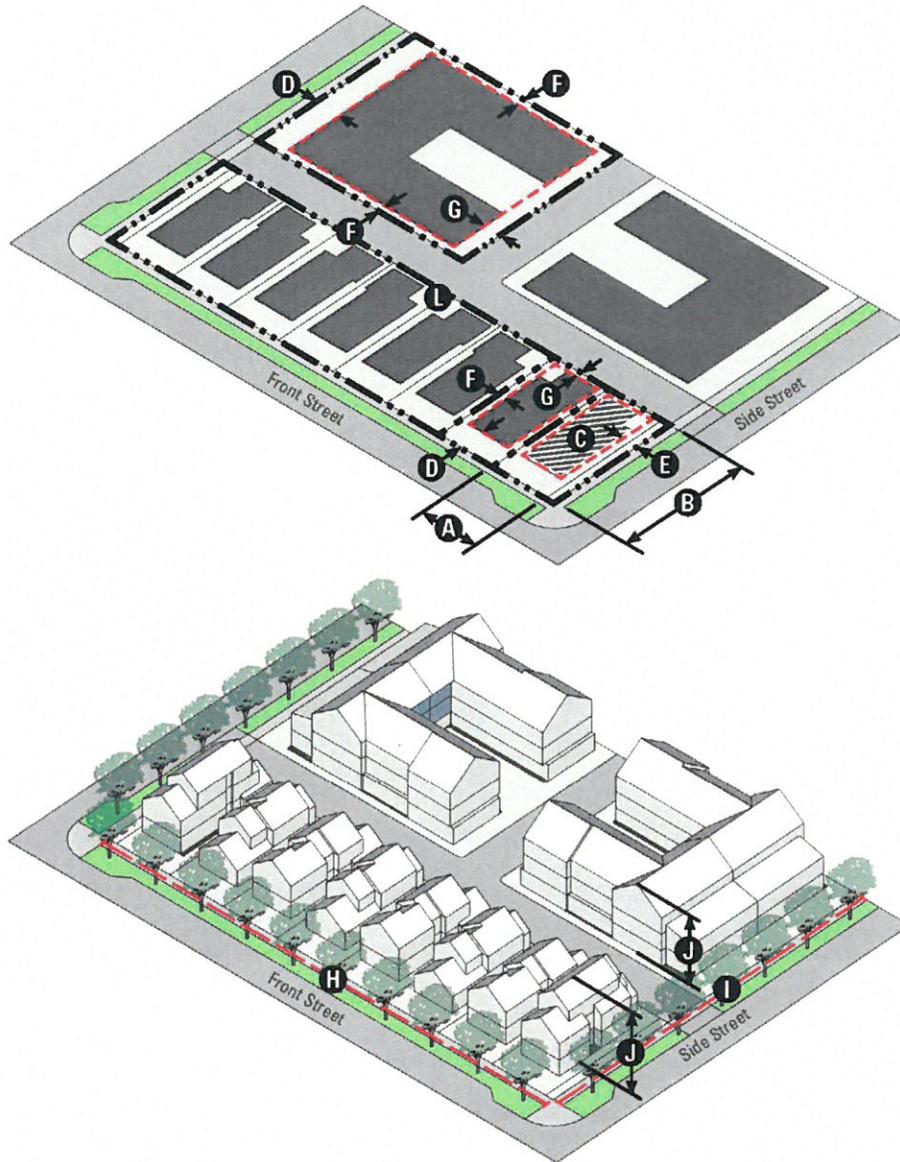
<b>Table 3-3 Mixed-Use 4 Parking Reductions</b>	
Amenity/Strategy	Standard Reduction
Park or plaza of at least 5% of the site area open to the public at least 8 hours per day.	15% reduction



**Figure 4: Mixed-Use 3 “Preferred” Development Standards Illustrations**

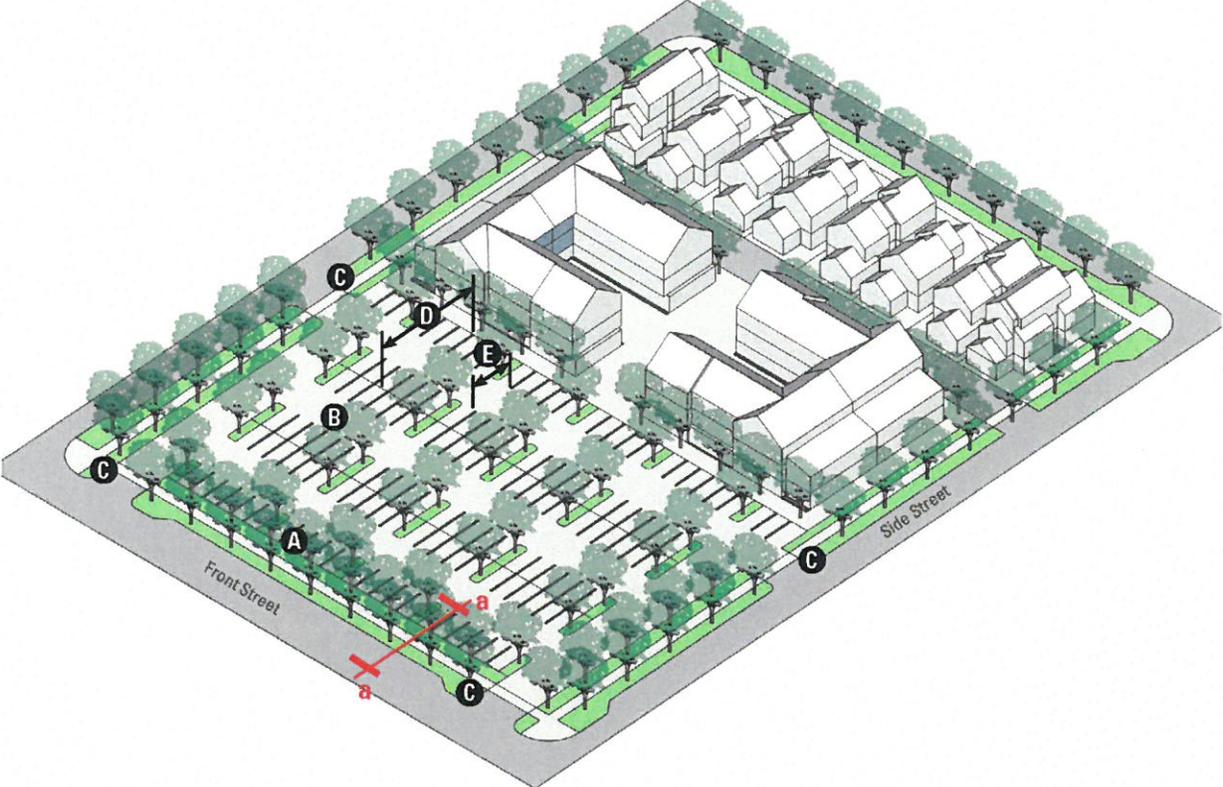
**DEVELOPMENT STANDARDS**

**MIXED-USE 3**

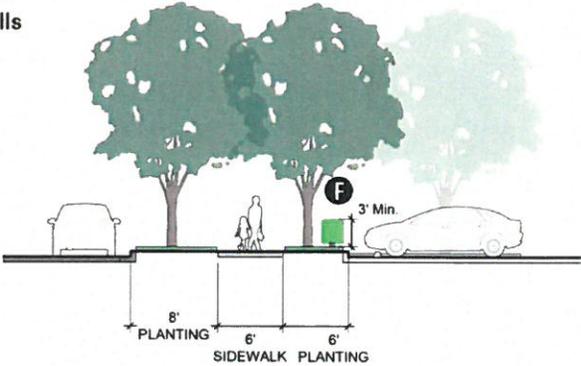


- |   |   |   |
|---|---|---|
| <b>A</b> Minimum Width: 30 feet min.                            | <b>E</b> Side Street Setback: 8 feet                                | <b>I</b> Building Façade at Build-to-Line: Side Street: 10 feet, 30% min. |
| <b>B</b> Minimum Depth: 80 feet min.                            | <b>F</b> Side Interior Setback: 5 feet/ 3 feet                      | <b>J</b> Building Height - Main Building: 3 floors max.                   |
| <b>C</b> Maximum Lot Coverage: 65%<br>Minimum Lot Coverage: n/a | <b>G</b> Rear Setback: 15 feet/ 3 feet                              | <b>L</b> Parking Lot Location   |
| <b>D</b> Front Setback: 15 feet                                 | <b>H</b> Building Façade at Build-to-Line: Front: 15 feet, 40% min. |   |

**Figure 4: Mixed-Use 3 “Optional” Development Standards Illustrations**



- A** Continued street trees, maximumu 30 feet apart
- B** Planter island with canopy trees every 6 stalls
- C** Parking lot entry/ exit
- D** Minimum 60 feet
- E** Minimum 24 feet
- F** Minimum 3 feet tall planting or wall



**SECTION a-a**

<b>Table 4-2 Mixed-Use 3 Parking Requirement Table</b>					
<b>Parking Requirements</b>					
<b>Land Use</b>	<b>Vehicular Parking</b>				<b>Bicycle Parking</b>
	<b>Minimum***</b>	<b>Maximum</b>	<b>Reductions Available or Alternative Requirements</b>	<b>Off-Site Parking</b>	<b>Minimum</b>
<b>Residential*</b>					
Single-Family Homes	2 spaces per unit	150% of minimum	See below	N/A	150% of minimum
Townhomes	1.5 spaces per unit				
Multi-Family	Studio/Efficiency: 0.95 spaces/unit 1-Bedroom: 1.25 space/unit 2-Bedroom and above: 1.75 spaces/unit			N/A	0.5 space per unit
Hotel/Motel	0.75 spaces/room plus 1 space per 1,000 sq. ft. meeting space	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 4 rooms
<b>Restaurant</b>					
Fast Food**	1 space per 100 sq. ft.	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 1,000 sq. ft.
All Other	1 space per 200 sq. ft.				
<b>Office</b>					
Medical	3 spaces per 1,000 sq. ft.	150% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft.
All Other	3 spaces per 1,000 sq. ft.				
General Retail & Service	2.5 spaces per 1,000 sq. ft.	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft.

\* On-street parking may be allowed to count towards requirement at the discretion of the Director of Planning and Development.

\*\* At least 2 queuing spaces are also required.

\*\*\* Parking minimums may include electric vehicle parking.

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements. Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

Parking Lot Location		Figure 4 "Preferred" Reference
Front	10 feet from Property Line	On-Street or Rear Access – "L"
Street Side	10 feet from Property Line	

**ALTERNATIVE MINIMUM PARKING REQUIREMENTS**

Use	Alternative Requirement
Independent Senior Living (55+)	0.75 spaces per unit
Assisted Senior Living (55+)	0.25 spaces per unit

**STANDARD PARKING REDUCTIONS**

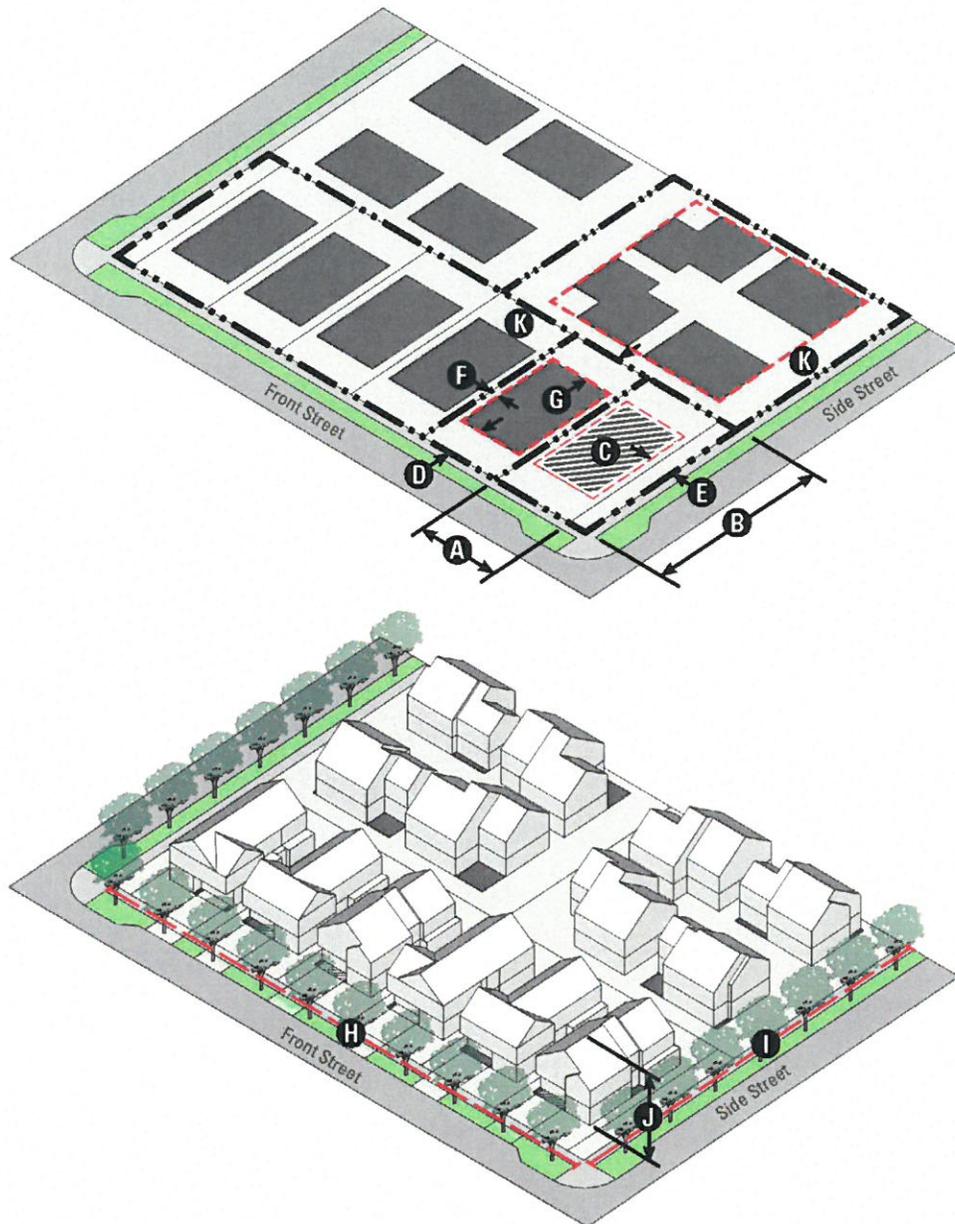
Standard reductions are available for developments that provide certain amenities or that implement certain strategies that provide alternatives to single-occupancy vehicles. Standard reductions can be granted administratively and deducted automatically from the applicable minimum parking requirement if the stated criteria are met, as determined by the Director of Planning and Development. These amenities and strategies are identified in Table 4-3 below.

Table 4-3 Mixed-Use 3 Standard Parking Reductions	
Amenity/Strategy	Standard Reduction
Park or plaza of at least 5% of the site area to the public at least 8 hours per day.	15% reduction

- (e) **Mixed-Use 2 Subdistrict.** The purpose of the Mixed-Use 2 Subdistrict is to provide a walkable, predominantly residential neighborhood that includes single-family and medium density building types within walking distance to transit and commercial areas. Commercial and mixed-use development are also permitted in this Subdistrict. The following Dimensional Tables (Tables 5-1, 5-2, and 5-3) apply to the Mixed-Use 2 Subdistrict. Graphics highlighting the dimensions follow in Figure 5.

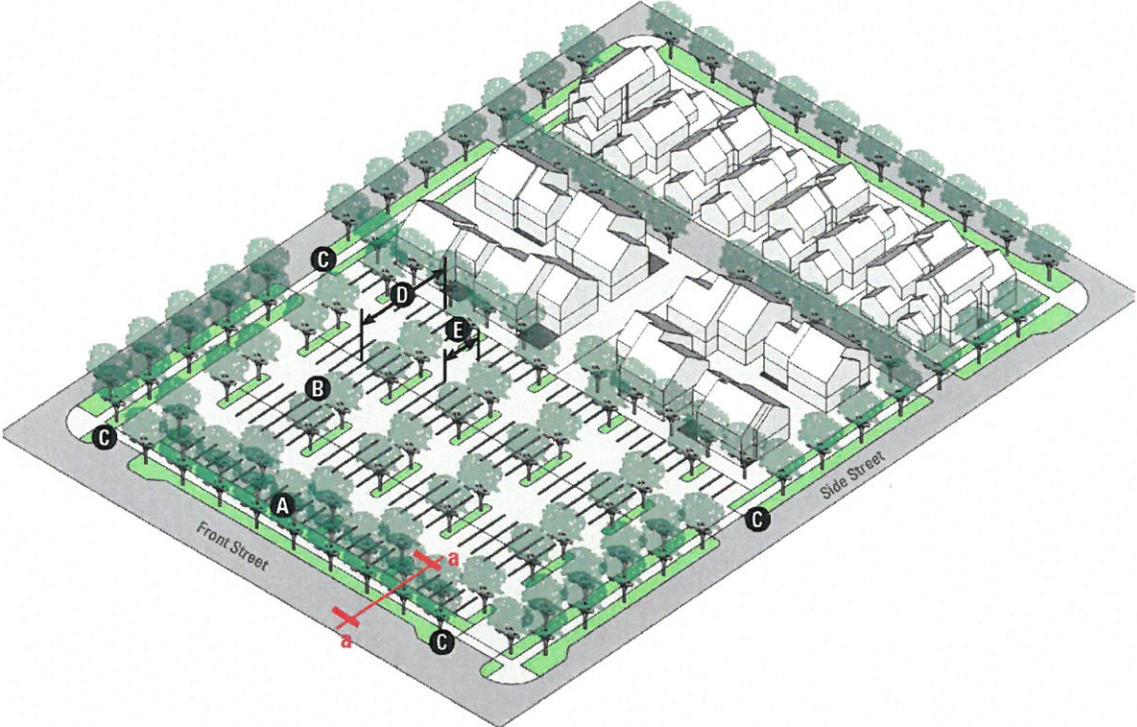
<b>Table 5-1 Mixed-Use 2 Dimensional Standards Table</b>			
<b>Lot Size</b>	<b>Lot Coverage</b>	<b>Building Placement</b>	<b>Building Height</b>
Minimum Width (A) 25 feet min.	Maximum (C) 60%	Front Setback (D)  15 feet for all streets except Highway 84 and Ross Clark Circle.  20 feet facing Highway 84 and Ross Clark Circle	Main Building (J) 2 floors max.
Minimum Depth (B) 80 feet min.	Minimum n/a	Side Street (E) 10 feet  20 feet facing Highway 84 and Ross Clark Circle Side Interior (F) To living 0 feet To alley loaded garage 0 feet  Rear (G) To living 15 feet To alley loaded garage 3 feet  Building Façade at Build-to-Line: Front (H) n/a Side Street (I) n/a	An additional floor is permitted if all parking is located behind the main building and screened from public rights-of-way.

**Figure 5: Mixed-Use 2 “Preferred” Development Standards Illustrations**

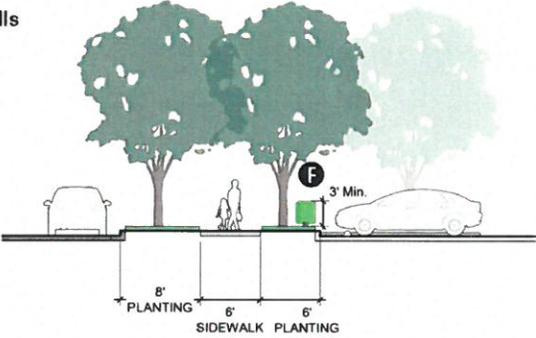


- A** Minimum Width: 25 feet min.
- B** Minimum Depth: 80 feet min.
- C** Maximum Lot Coverage: 60%  
Minimum Lot Coverage: n/a
- D** Front Setback: 15 feet
- E** Side Street Setback: 10 feet
- F** Side Interior Setback: 0 feet
- G** Rear Setback: 15 feet/ 3 feet
- H** Building Façade at Build-to-Line:  
Front: n/a
- I** Building Façade at Build-to-Line:  
Side Street: n/a
- J** Building Height - Main Building:  
2 floors max.
- K** Parking Lot Location

**Figure 5: Mixed-Use 2 “Optional” Development Standards Illustrations**



- A** Continued street trees, maximum 30 feet apart
- B** Planter island with canopy trees every 6 stalls
- C** Parking lot entry/ exit
- D** Minimum 60 feet
- E** Minimum 24 feet
- F** Minimum 3 feet tall planting or wall



**SECTION a-a**

<b>Table 5-2 Mixed-Use 2 Parking Requirement Table</b>					
<b>Parking Requirements</b>					
Land Use	Vehicular Parking				Bicycle Parking
	Minimum**	Maximum	Reductions Available or Alternative Requirements	Off-Site Parking	Minimum
Residential*					
Single-Family Homes	2 spaces per unit	150% of minimum	See below	N/A	No requirement
Townhomes	1.5 spaces per unit				
Multi-Family	Studio/Efficiency: 1 space/unit 1-Bedroom: 1.4 space/unit 2-Bedroom and above: 2.0 spaces/unit			N/A	0.5 space per unit

\* On-street parking may be allowed to count towards requirement at the discretion of the Director of Planning & Development.

\*\* Parking minimums may include electric vehicle parking.

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements. Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

<b>Parking Lot Location</b>	<b>Figure 5 “Preferred” Reference</b>
Front	On-Street or Rear Access - K
Street Side	

**ALTERNATIVE MINIMUM PARKING REQUIREMENTS**

<b>Use</b>	<b>Alternative Requirement</b>
Independent Senior Living (55+)	0.75 spaces per unit
Assisted Senior Living (55+)	0.25 spaces per unit

**STANDARD PARKING REDUCTIONS**

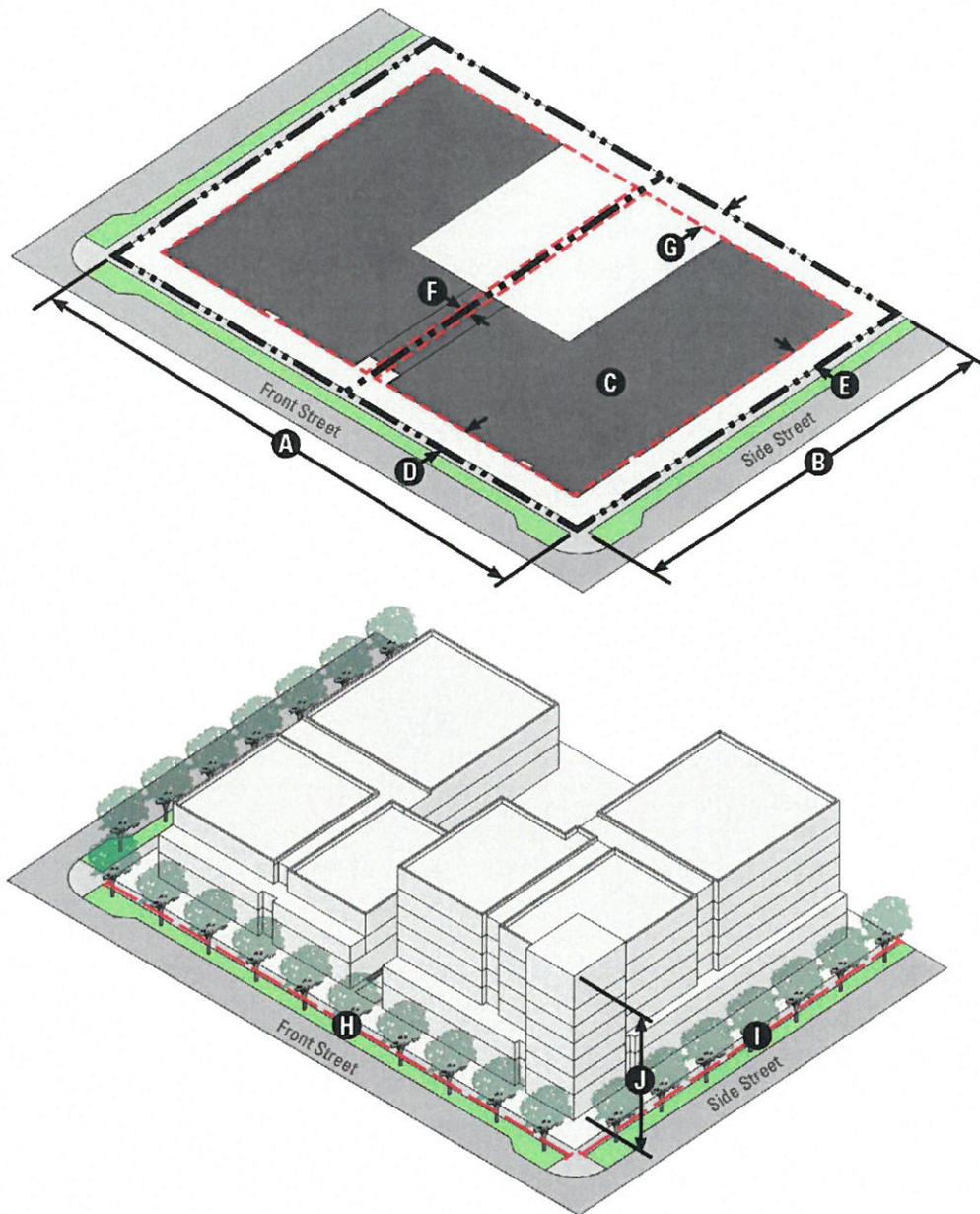
Standard reductions are available for developments that provide certain amenities or that implement certain strategies that provide alternatives to single-occupancy vehicles. Standard reductions can be granted administratively and deducted automatically from the applicable minimum parking requirement if the stated criteria are met, as determined by the Director of Planning and Development. These amenities and strategies are identified in Figure 5-3 below.

<b>Table 5-3 Mixed-Use 2 Standard Parking Reductions</b>	
Amenity/Strategy	Standard Reduction
Park or plaza of at least 1,000 sq. ft, open to the public at least 8 hours per day that is provided in addition to park requirement.	15% reduction

- (f) Institutional. The purpose of the Institutional Subdistrict is to provide appropriately scaled development that contributes to a pedestrian-oriented built environment while also meeting the needs of the community these districts serve. This Subdistrict recognizes the inherent challenges that large institutions and special use districts face in adhering to the development standards of the other Subdistricts. Therefore, a separate Subdistrict is identified to establish standards specific to these types of uses. The following Dimensional Tables (Tables 6-1, 6-2, and 6-3) apply to the Institutional Subdistrict.

<b>Table 6-1 Institutional Dimensional Standards Table</b>				
Lot Size	Lot Coverage		Building Placement	Building Height
Minimum Width (A) N/A	Maximum (C)	65%	Front Setback (D)  15 feet for all streets except Highway 84 and Ross Clark Circle.  20 feet facing Highway 84 and Ross Clark Circle	Main Building (J) 7 Floors
Minimum Depth (B) N/A	Minimum	n/a	Side Street (E) 15 feet  20 feet facing Highway 84 and Ross Clark Circle Side Interior (F) 15 feet  Rear (G) 15 feet  Building Façade at Build-to-Line: Front (H) N/A Side Street (I) N/A	

**Figure 6: Institutional Development Standards Illustrations**



- |   |   |   |
|---|---|---|
| <b>A</b> Minimum Width: n/a                                     | <b>E</b> Side Street Setback: 15 feet                 | <b>I</b> Building Façade at Build-to-Line: Side Street: n/a |
| <b>B</b> Minimum Depth: n/a                                     | <b>F</b> Side Interior Setback: 15 feet               | <b>J</b> Building Height - Main Building: 7 floors          |
| <b>C</b> Maximum Lot Coverage: 65%<br>Minimum Lot Coverage: n/a | <b>G</b> Rear Setback: 15 feet                        |   |
| <b>D</b> Front Setback: 15 feet                                 | <b>H</b> Building Façade at Build-to-Line: Front: n/a |   |

<b>Table 6-2 Institutional Parking Requirement Table</b>					
<b>Parking Requirements</b>					
<b>Land Use</b>	<b>Vehicular Parking</b>				<b>Bicycle Parking</b>
	<b>Minimum**</b>	<b>Maximum</b>	<b>Reductions Available or Alternative Requirements</b>	<b>Off-Site Parking</b>	<b>Minimum</b>
Assembly	1 space per 250 sq. ft.	125% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft. of main assembly area
Educational					
Elementary, Junior High, Middle Schools	0.15 spaces per student calculated based on intended maximum student capacity of facility	150% of minimum	See below	Up to 1,500 ft ADA-accessible walking path	0.05 spaces per student calculated based on intended maximum student capacity of facility
High Schools	0.30 spaces per student calculated based on intended maximum student capacity of facility				0.05 spaces per student calculated based on intended maximum student capacity of facility
Post-Secondary	3.75 spaces per 1,000 sq. ft.				1 space per 1,000 sq. ft.
Hospital	1 space per 5 beds + 3 spaces per 1,000 sq. ft. Medical Office Building Space.	150% of minimum	See below	Up to 1,000 ft ADA-accessible walking path	1 space per 2,000 sq. ft. of office space
All Other Institutional Uses*	At the discretion of the Director of Planning and Development				

\* On-street parking may be allowed to count towards requirement at the discretion of Director of Planning & Development.

\*\* Parking minimums may include electric vehicle parking.

*Non-Conforming Properties. If a property is nonconforming as to off-street parking requirements, only additions to the nonconforming structure shall be required to comply with off-street parking requirements.*

*Existing legally established non-conformities related to off-street parking may continue in accordance with Section 114-171 through 114-173.*

**ALTERNATIVE MINIMUM PARKING REQUIREMENTS**

Use	Alternative Requirement
Independent Senior Living (55+)	0.75 spaces per unit
Assisted Senior Living (55+)	0.25 spaces per unit

**STANDARD PARKING REDUCTIONS**

Standard reductions are available for developments that provide certain amenities or that implement certain strategies that provide alternatives to single-occupancy vehicles. Standard reductions can be granted administratively and deducted automatically from the applicable minimum parking requirement if the stated criteria are met, as determined by the Director of Planning and Development. These amenities and strategies are identified in Table 6-3 below.

<b>Table 6-3 Institutional Standard Parking Reductions</b>	
Amenity/Strategy	Standard Reduction
Park or plaza of at least 1,000 sq. ft, open to the public at least 8 hours per day that is provided in addition to park requirement.	15% reduction

**Section 114-163. Uses Permitted**

(a) Land Use Tables. Permitted Uses, Special Exception Uses, and Not Permitted Uses are outlined in the following Tables.

(1) Accessory Uses and Structures.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown Core	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Accessory Uses/ Structures</b>						
Accessory Dwelling Unit	S	S	S	S	S	S
Accessory Dwelling Unit (For Owner/Operator)	-	-	-	-	S	S
Accessory Structures (Sheds, Garages, etc.)	P	P	P	P	P	P
Accessory Non-Residential Use	-	-	P	P	P	P
Amenity Center/ Clubhouse	P	P	P	P	P	P
Billboards	p <sup>(1)</sup>	p <sup>(1)</sup>	p <sup>(1)</sup>	p <sup>(1)</sup>	-	-
Private Cemetery (accessory to a church)	S	S	S	S	S	S
Daycare, Child/Adult (In-Home less than 6)	S	S	S	S	S	S
Fences, Walls	P	P	P	P	P	P
Home Occupation, Non-qualifying	S	S	S	S	S	S
Home Occupation, Qualifying	P	P	P	P	P	P
Open Outdoor Storage of Goods or Materials	-	-	-	-	-	-
Portable Signs	-	-	-	-	-	-
Shelter for Livestock (Stables)	-	-	-	-	-	-
Truck/Trailer Rental (Accessory Use)	-	-	-	-	-	-

(2) Agricultural Uses.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown Core	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Agricultural Uses</b>						
Aircraft Landing Field	-	-	-	-	-	-
Agriculture/Poultry/ Livestock Raising (As Primary Use)	-	-	-	-	-	-
Private Animal Shelter or Rescue	-	-	-	-	-	-
Auction (Livestock/ Equipment)	-	-	-	-	-	-
Aviary/ Apiary	-	-	-	-	-	-
Community Garden	S	S	S	S	S	S
Forestry	-	-	-	-	-	-
Gardening (As Accessory Use)	-	-	-	-	-	-
General Farming/ Horticulture	-	-	-	-	-	-
Hatchery, Aquaculture	-	-	-	-	-	-
Mining (Natural Resources Indigenous to Area)	-	-	-	-	-	-
Nursery/ Plant Sales (Wholesale)	-	-	-	-	-	-
Poultry/Livestock (As Accessory Use)	-	-	-	-	-	-
Riding Academy	-	-	-	-	-	-
Roadside Stand (Farm Produce)	-	-	-	-	-	-
Stable	-	-	-	-	-	-
Taxidermy	-	-	-	-	-	-
Temporary/ Seasonal Use	-	-	-	-	-	-

## (3) Commercial Uses.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown Core	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Commercial Uses</b>						
Accessory Building Sales	-	-	-	-	-	-
Adult Entertainment	-	-	-	-	-	-
Alcohol Beverage Establishment (Primary Use)	P	P	P	P	P	P
Amusement Commercial (Inside)	P	P	P	P	P	P
Amusement Commercial (Outside)	S	S	S	S	S	S
Animal Boarding	-	-	-	-	-	-
ATM	P	P	P	P	P	P
Automobile Parking (Lot or Structure)	S	S	P	P	P	P
Automobile Repair/Service	S	S	P	P	P	P
Automobile/ Motor vehicle Sales, New or Used	-	-	-	-	-	-
Automobile/ Motor vehicle Rental	-	-	-	-	-	-
Automobile Wash	-	-	-	-	-	-
Bank/ Financial Institution	P	P	P	P	P	P
Bed and Breakfast Inn	P	P	P	P	P	P
Bus Station	-	-	-	-	-	-
Butcher (Retail)	P	P	P	P	P	P
Campground/ RV Park	-	-	P	P	P	P
Caterer	P	P	P	P	P	P
Club (Fraternal)	P	P	P	P	P	P
Commissary (Health Dept. approval Req)	P	P	P	P	P	P
Contractor Office/ Yard (building, plumbing, electrical, etc.)	-	-	-	-	-	-
Convenience Store	S	S	S	S	S	S
Country Club	-	-	-	-	P	-
Curb Market	-	-	S	S	S	S
Custom Assembly	P	S	S	S	S	-

Donation Center, Drop-Off Box	P	P	P	P	P	P
Distribution Center (UPS, FedEx, Newspaper, etc.)	-	-	-	-	-	-
Driving Range	-	-	-	-	-	S
Dry Cleaning & Laundry Establishment <3,000 sq ft	P	P	P	P	P	P
Flea Market	-	-	-	-	-	-
Funeral Home	P	P	P	P	P	P
Golf Course	-	-	-	-	-	-
Grocery Store	P	P	P	P	P	P
Gymnasium/ Health Club	P	P	P	P	P	P
Home Improvement Sales	S <sup>(2)</sup>					
Hotel/Motel	P	P	P	P	P	P
Kennel	-	-	-	-	-	-
Live/Work	P	P	P	P	P	P
Lodges, Summer Camps	-	-	-	-	-	-
Major Appliance Repair	-	-	-	-	-	-
Manufacturing Incidental to Retail on Premises	S	S	S	S	S	S
Manufactured Homes Sales	-	-	-	-	-	-
Microbrewery	P	P	P	P	P	P
Mobile Accessory Storage Containers (sale or rent)	-	-	-	-	-	-
Mobile Vendor	P	P	P	P	P	P
Moving Truck & Trailer Rental (Principal Use)	-	-	-	-	-	-
Multi-Media Production (TV or Radio Station)	P	P	P	P	P	P
Museum/ Art Gallery (private or semi-public)	P	P	P	P	P	P
Office (General)	P	P	P	P	P	P
Pawn Shop	-	-	-	-	-	-
Personal Care Services	P	P	P	P	P	P
Personal Instruction	P	P	P	P	P	P
Printing and Publishing	P	P	P	P	P	P
Racetrack	-	-	-	-	-	-
Restaurant, Carry-Out/ Take-Out/ Delivery	P	P	P	P	P	P
Restaurant, Fast-Food	-	-	S	S	S	S
Restaurant, Full-Service	P	P	P	P	P	P

Retail, Other	P	P	P	P	P	P
Retail, Indoor Sales Only	P	P	P	P	P	P
Self-Service Laundry	P	P	P	P	P	P
Self-Service (Mini) Storage	-	-	-	-	-	-
Shooting Range (Indoor)	-	-	-	-	-	-
Shooting Range (Outdoor)	-	-	-	-	-	-
Small Appliance Repair	-	-	-	-	-	-
Specialty Retail	P	P	P	P	P	P
Stadium/Arena/ Convention Center	P	P	P	P	P	P
Taxi Dispatch, Excluding Garage	P	P	P	P	P	P
Telecommunications Tower	P	P	P	P	P	P
Temporary/ Seasonal Use	P	P	P	P	P	P
Theater (Indoor)	P	P	P	P	P	P
Theater (Outdoor)	P	P	P	P	P	P
Title Loans, Payday Lender, Check Cashing (deferred presentment)	-	-	-	-	-	-
Utility Trailer Sales or Rental	-	-	-	-	-	-
Wrecker Service	-	-	-	-	-	-
Zoo	-	-	-	-	-	-

## (4) Industrial Uses.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown Core	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Industrial Uses</b>						
Airport/ Heliport	-	-	-	S	S	S
Building Contractor Supply	-	-	-	-	-	-
Construction/ Demolition Landfill, Private	-	-	-	-	-	-
Food Processing Excluding Vinegar and Yeast	-	-	-	-	-	-
Food and Beverage Production Including Rendering	-	-	-	-	-	-
Hazardous Operation	-	-	-	-	-	-
Heavy Equipment Sales and Service	-	-	-	-	-	-
Laundry/ Dry Cleaning Plant, Dyeing, etc. <3,000 sq ft	-	-	-	-	-	-
Manufacturing, Heavy	-	-	-	-	-	-
Manufacturing, Light (with incidental retail)	-	-	-	-	-	-
Meat, Fish, Poultry Processing Excluding Slaughter	-	-	-	-	-	-
Mineral Extraction	-	-	-	-	-	-
Motor Freight	-	-	-	-	-	-
Nursery, Plant Sales/Landscape Materials (Retail)	-	-	-	-	-	-
Nursery, Plant Sales/ Landscape Materials (Wholesale)	-	-	-	-	-	-
Office Warehouse	-	-	-	-	-	-
Railroad Station	-	-	-	-	-	-
Railroad Yard	-	-	-	-	-	-
Recycling Collection Center	-	-	-	-	-	-
Recycling Facility	-	-	-	-	-	-
Scrap Operation (Junk Yard)	-	-	-	-	-	-

Slaughterhouse	-	-	-	-	-	-
Transit Garage (Bus, Van, Taxi, etc.)	-	-	-	-	-	-
Tank Farm	-	-	-	-	-	-
Warehouse	-	-	-	-	-	-

(5) Institutional Uses.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown Core	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Institutional Uses</b>						
Cemetery/ Mausoleum	-	-	-	-	-	-
Correctional Facility	-	-	-	-	-	-
Community Center	S	S	S	S	S	S
Daycare Center More than 6 (Child or Adult)	P	P	P	P	P	P
Fraternity/ Sorority House (off campus)	-	-	-	P	P	P
Group Home/ Shelter Home	P	P	P	P	P	P
Halfway/ Transitional Housing	-	-	-	-	-	-
Crematory	-	-	-	-	-	-
Public Uses (Police, Fire, Utilities, Public Works, etc.)	P	P	P	P	P	P
Religious Institution	P	P	P	P	P	P
School, College/ University	P	P	P	P	P	P
School, Technical/ Vocational/ Business	P	P	P	P	P	P
Semi-public land use	P	P	P	P	P	P
Transient (Homeless) Shelter	-	-	-	-	-	-

(6) Medical and Residential Uses.

Legend: P = Permitted S = Special Exception - = Not Permitted	Subdistricts					
	Downtown	Downtown Edge	MU-4	MU-3	MU-2	Institutional
<b>Medical Uses</b>						
Animal Hospital/ Veterinarian	p <sup>(3)</sup>					
Assisted Living Facility	P	P	P	P	P	P
Detoxification Clinic	-	-	-	-	-	-
Hospital	-	-	P	P	P	P
Medical Office	P	P	P	P	P	P
Medical or Scientific Lab	P	P	P	P	P	P
Nonresidential Drug Treatment Facility	-	-	-	-	-	-
Nursing Home	P	P	P	P	P	-
Outpatient Clinic (Including Surgery)	P	P	P	P	P	P
Rehabilitation Facility	P	P	P	P	P	P
<b>Residential Uses</b>						
Apartments (8 or more units)	P	P	P	P	P	P
Live/Work	P	P	P	P	P	P
Manufactured Single-Family Home	-	-	-	-	-	-
Single Family Dwelling	P	P	P	P	P	P
Two-Family Dwelling	P	P	P	P	P	P
Three-, Four-, Five-, Six-, Seven-Family Dwelling	P	P	P	P	P	P
<sup>(1)</sup> Existing digital and static billboards are permitted uses. No new billboards are permitted in the Downtown Core and Downtown Edge Subdistricts. New static display billboards permitted in the MU-3 and MU-4 subdistricts per provisions in 114-215						
<sup>(2)</sup> Not to exceed 50,000 sq ft						
<sup>(3)</sup> Special Exception is required if non-medical boarding is included						

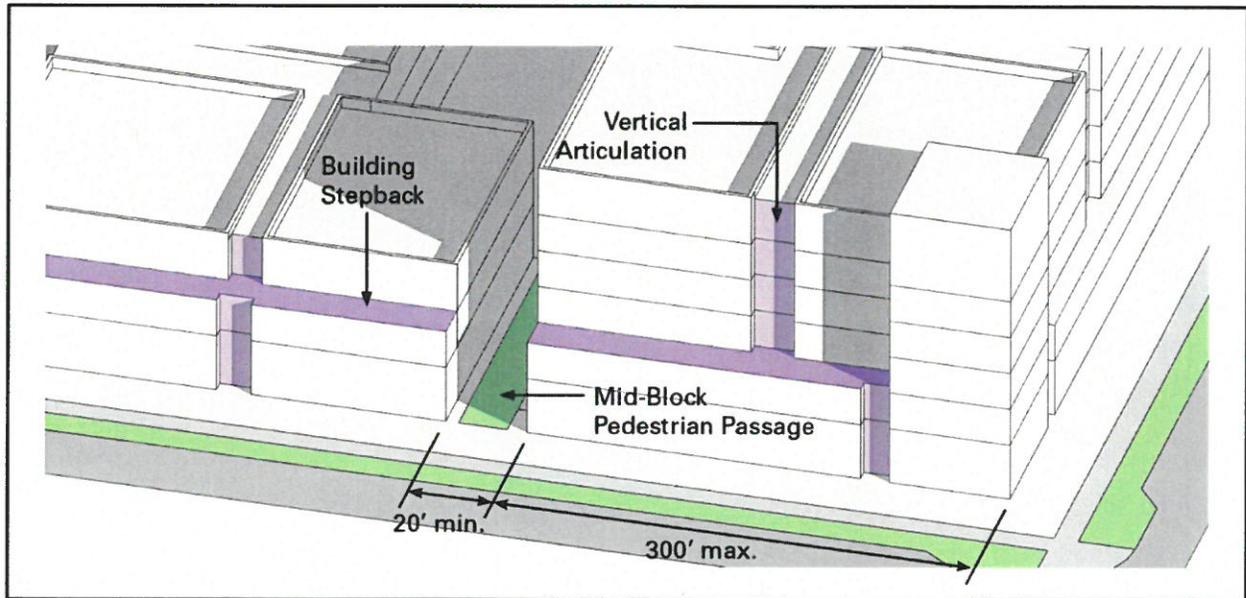
**Section 114-164. General Standards.** These are standards / requirements pertain to all development in the Overlay unless specific Subdistricts are identified within. To implement the Highway 84 East Vision, the following development standards apply to all development in the Overlay unless specific Subdistricts are identified within.

(a) Architectural Standards.

(1) Massing.

- a. In the Downtown Core and Downtown Edge, buildings shall maintain a consistent street wall along all their street frontages. Variety in massing may occur through step-backs on upper floors.
- b. In the Mixed-Use 4 and Mixed-Use 3, buildings shall maintain a consistent street wall along all streets except for Highway 84. Variety in massing may occur through step-backs on upper floors.
- c. Breaks in the street wall shall be limited to those necessary to accommodate pedestrian pass-throughs, public plazas, entry forecourts, permitted vehicular access driveways, and hotel drop-offs.
- d. Large projects should be broken into a series of appropriately scaled buildings for pedestrian scale and walkability. A passageway of at least twenty (20) feet wide should be located between buildings so that no building is more than three hundred (300) feet in length. The passageway should be clearly visible from the street, provide a clear connection to adjacent common areas, and provide enhanced landscaping, materials, and lighting to create a pleasant pedestrian experience. Monolithic slab-like structures that wall off views and overshadow the surrounding neighborhood are discouraged. See Figure 7.

**Figure 7: Massing, Step Back and Passageway**



**(2) Architectural Detail.**

- a. Buildings should be well-detailed with long-lived materials that can be appreciated when viewed by the pedestrian.
- b. Architectural features should be layered to emphasize certain features of the building such as entries, corners, common open spaces, public amenity spaces, and organization of units.

- c. Building entries shall be designed as the focal point of the building façade. Accentuate main building entries by creating a vertical break in the building wall.
- d. Windows and doors shall be used as character-defining features to reflect an architectural style or theme consistent with other façade elements. Windows should project or be inset from the exterior building wall and incorporate well-designed trims and details. Use balconies, fenestration, or other elements to create an interesting pattern of projections and recesses.



*The of variety windows and balconies create visual interest*

- e. Vary the plane of a building to provide visual interest and contribute to the quality and definition of the street wall. Apply horizontal architectural variations to break down the scale and massing of longer facades.



*Variety of building massing helps to break down the scale of the buildings*

- f. Employ a different architectural treatment on the ground floor façade than on the upper floors. Provide an identifiable break between the building's

base and mid-section with a change in material, change in fenestration pattern, or similar means.

- g. The cornice or roof line of historic structures shall be reflected with a demarcation on new adjacent structures. Similarly, the fenestration and pattern of historic buildings shall be reflected on new adjacent structures.
- h. Where appropriate, employ shade and shadow created by reveals, surface changes, overhangs, and sunshades to provide visual interest on façades exposed to the sun and refuge for pedestrians.

(b) Materials.

- (1) The interplay of materials, windows and other elements should support the larger design objectives as articulated by the architect.
- (2) Buildings shall aim for a “timeless design” and employ high-quality, durable materials and careful detailing that have proven longevity.
  - a. Feature long-lived and sustainable materials. The material palette should provide variety, reinforce massing and changes in the horizontal or vertical plane.
  - b. Use durable materials on ground floor façades. Low-quality materials such as stucco, plaster, and Exterior Insulation Finishing System (EIFS) are prohibited at the ground-floor along any streets, alleys, or public amenity spaces.
  - c. Use especially high-quality durable materials on upper floor facades. Low-quality materials such as stucco, plaster, and EIFS are not permitted on exterior building facade facing streets, alleys, or public amenity spaces, except as minor detail components amounting to no more than ten percent (10%) of any elevations above the ground floor.
  - d. To provide visual variety and depth, layer the building materials to provide a variety of textures that bear a direct relationship to the building’s massing and structural elements. The materials should reinforce the integrity of the design concept and the building’s structural elements, and not appear as an imitation of a material or style.



*High quality building materials and simple architectural design*

(c) Windows and Doors.

- (1) Provide high-performance, well-detailed windows and doors that add to the depth and scale of the building's façade.
- (2) Window placement, size, material, and style should help define a building's architectural style and integrity.
- (3) In buildings other than curtain wall buildings, windows shall be recessed from the exterior building wall, except where inappropriate to the building's architectural style. Generally, the required recess may not be accomplished using plants around the window.



*Windows recessed from front building elevation to create visual interest*

- (4) Windows and doors shall be well-detailed where they meet the exterior wall to provide adequate weather protection and to create a shadow line.

(d) Maximum Block Size.

- (1) Maximum Perimeter Length: 2200 feet, unless constrained by location conditions. Information about the specific constraint must be included with the development application and the review will be completed by the Development Review Committee (DRC).
- (2) Maximum Block Length: 700 feet
- (3) Maximum Block Area: 4.0 acres

(e) Building Orientation.

- (1) Buildings shall be oriented toward streets with parking located away from primary streets. In the Mixed-Use 4, 3, 2, and Institutional Subdistricts a parking lot can front Highway 84 and Ross Clark Circle under the "Optional" alternative for building placement.
- (2) The principal entrances for buildings shall be oriented toward primary streets.



*Building entrances fronting on streets*

(f) Public Realm and Open Area Standards.

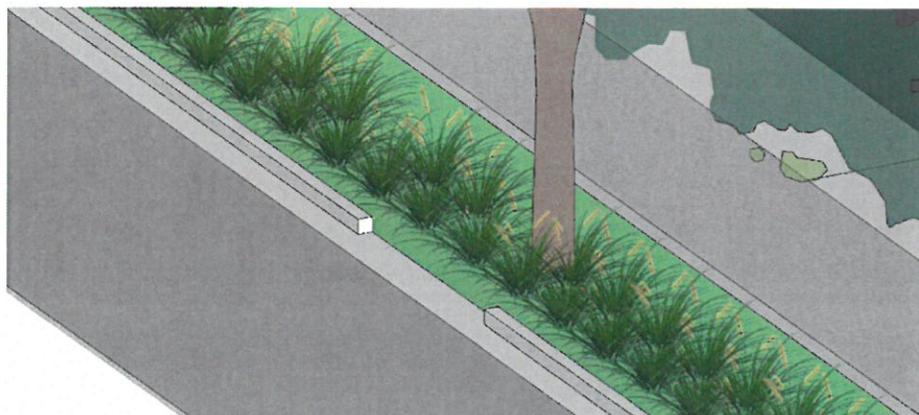
- (1) Design flexible outdoor public spaces that can support a range of uses as public gathering places including seating, conversing, window-shopping, and dining, playing, or special events programming such as farmers markets and art exhibits.
- (2) Where blocks are longer than four hundred (400) feet, one mid-block pedestrian pathway, which is open to the public, should be provided to enhance walkability of streets and neighborhoods.
- (3) All outdoor public spaces shall provide ADA-compliant walkways to ensure ease of access for all users.

- (4) Open-air outdoor spaces shall be landscaped to provide shade, aesthetics, and comfort for users. Planters, planter boxes and similar planting containers may count toward this requirement.
- (5) Non-movable or fixed seating should be placed with consideration to noontime sun and shade; deciduous trees should be planted as the most effective means of providing comfortable access to sun and shade.
- (6) Above-grade outdoor spaces including roof or podium terraces, building cut-outs, or residential courtyards, shall incorporate trees and other plantings in permanent or temporary planters that will shade, reduce reflective glare, and add interest to the space.
- (7) Landscape elements should establish scale and reinforce continuity between indoor and outdoor space. Canopy trees shall be provided within open spaces, especially along streets and required setbacks. Tree species selected should provide a minimum of thirty-five percent (35%) tree canopy coverage for the landscaped areas within the site. The site landscape plan shall annotate the tree species, placement on site, and canopy dimensions. Mature canopy dimensions to be shown graphically on the site landscape plan.
  - a. Landscape elements should provide scale, texture, and color. A rich, coordinated palette of landscape elements that enhances the site's identity is encouraged.

(g) Stormwater Plan.

- (1) Stormwater shall be detained and retained in a way that will support overall drainage and will not shift impacts into the public right-of-way or onto another property.
- (2) All new development shall utilize bioswales, as opposed to traditional detention and retention ponds, to the extent practical. Bioswales are required, to the extent practical, on new streets and in parking lots. Bioswales are engineered elements in the hardscape that divert stormwater from parking lots and streets into vegetated landscaped elements such as parking islands, buffered areas, and median swales. See Figure 8.

**FIGURE 8 – Bioswale adjacent to street.**



- (3) When a detention pond is used and is visible from a public right-of-way, it must be designed to serve as a naturalized landscape features and include a landscape planting plan.
- (4) A coordinated stormwater plan between adjacent property owners is encouraged so the location of retention / detention facilities is efficient and in the most appropriate location.



## (h) Signage Standards.

## (1) General Requirements.

- a. All signs erected in the Overlay shall comply with the requirements in Article XII, Signs and Billboards, in addition to the requirements outlined here in.
- b. To the extent that anything in this Chapter conflicts with Article XII, this Chapter shall prevail.
- c. The following sign standards in this Chapter apply to the Overlay Subdistricts:
  1. Section 114-217 (Sign standards in residential districts) applies in the Mixed-Use 3 and Mixed-Use 2 Subdistricts;
  2. Section 114-218 (Sign standards for office and institutional districts) applies in the Downtown Core, Downtown Edge, and Mixed-Use 4 Subdistricts; and
  3. Section 114-219 (Sign standards for commercial and industrial districts) applies in the Institutional Subdistrict.
- d. The Overlay sign requirements are intended to promote signage that is complimentary and supportive of surrounding development and the goals of the Highway 84 East Overlay.
- e. Signage is to be developed with a pedestrian focus, as outlined in the Highway 84 Vision Plan, instead of vehicular focus.
- f. The Overlay sign requirements are intended to promote signage that is aesthetically pleasing and that supports the overall character of the City of Dothan. Signs and lighting should not promote visual clutter.

## (2) Prohibited Signs.

- a. In addition to the sign types prohibited in Article XII, the following sign types are prohibited in the Overlay.
- b. Portable signs and inflatable devices, which includes devices consisting of flexible material that is shaped by air or other gas, whether stationary or featuring movement.
- c. Vehicle signs. Signs placed on or affixed to vehicles and/or trailers, including bicycles, which are parked on a public right-of-way, public property, or private property to be visible from a public right-of-way, unless such vehicle is used in the normal day-to-day operations of the business. This is not in any way intended to prohibit signs placed on or affixed to vehicles and trailers, such as lettering on motor vehicles, where the sign is incidental to the primary use of the vehicle or trailer for transportation.
- d. Temporary Banners.
  1. Pole Signs.

## (3) Mounting and Placement.

- a. Signs shall be mounted in such a manner as to not obscure architectural features and openings of a building.
- b. Signs shall not be placed higher than the cornice line.
- c. Basement and Ground Floor businesses are limited to placing signs on the Ground Floor and no higher than fifteen (15) feet above grade.

## (4) Materials.

- a. Sign materials shall be sturdy and generally match the character of the building.
- b. Plastic, vinyl, and similar materials are prohibited.

(5) Signage Lighting.

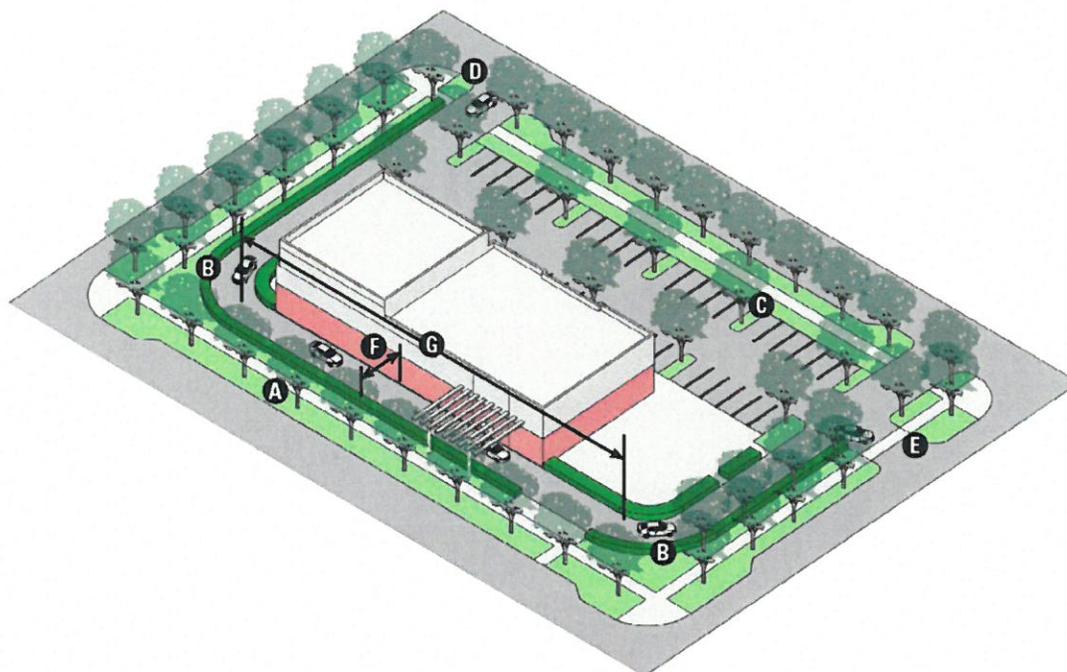
- a. Signage illumination is to aid in navigation while also keeping a focus on public safety and promoting the aesthetic and visual character of the community.
- b. Illumination of signs for non-residential uses in residential districts requires Special Exception approval.

(6) Billboards.

- a. West of the Columbia Highway / Highway 84 intersection to Park Avenue.
  - 1. All existing billboards permitted by right. No new static billboards shall be permitted.
  - 2. With the exception of the Plant Street / Highway 84 intersection, one additional digital billboard may be permitted between the AL 52 (Columbia Highway) / US 84 intersection and the Park Avenue / US 84 intersection.
  - 3. In exchange for the installation of an additional digital billboard, three (3) static billboards located between Columbia Highway and Park Avenue shall be removed.

(i) Drive-Through Design Standards.



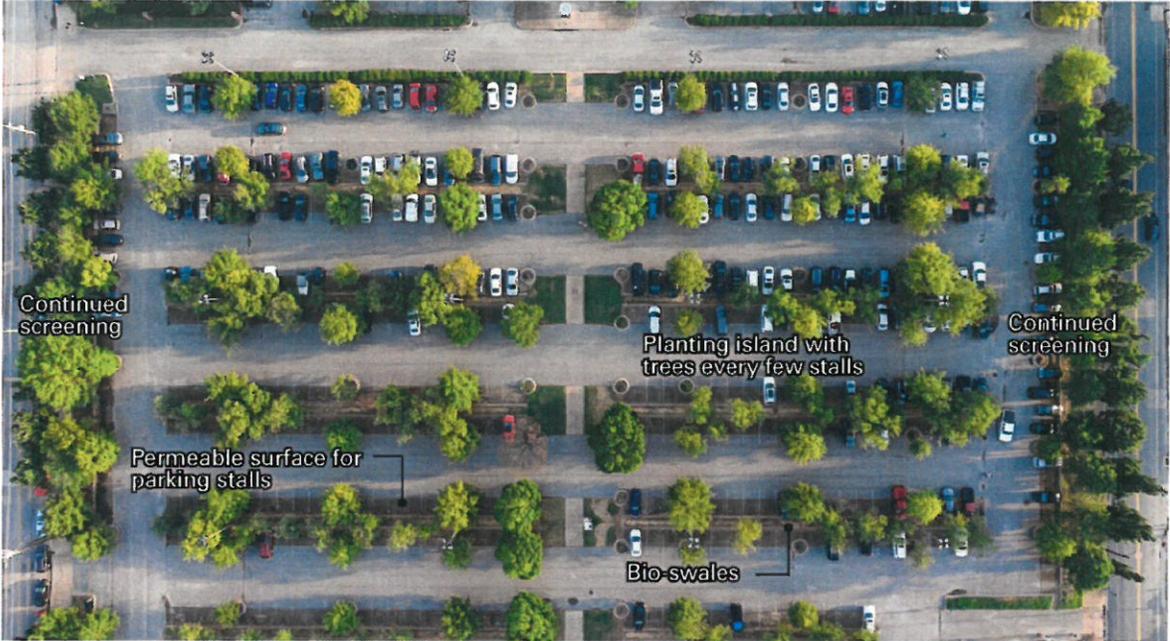


- A** Continued street trees, maximum 30 feet apart
- B** Screening, minimum 3 feet tall
- C** Planter island with canopy trees every 6 stalls
- D** Drive-through entry, minimum 24 feet width
- E** Drive-through exit
- F** Minimum 12 feet per lane
- G** Minimum 36 feet length

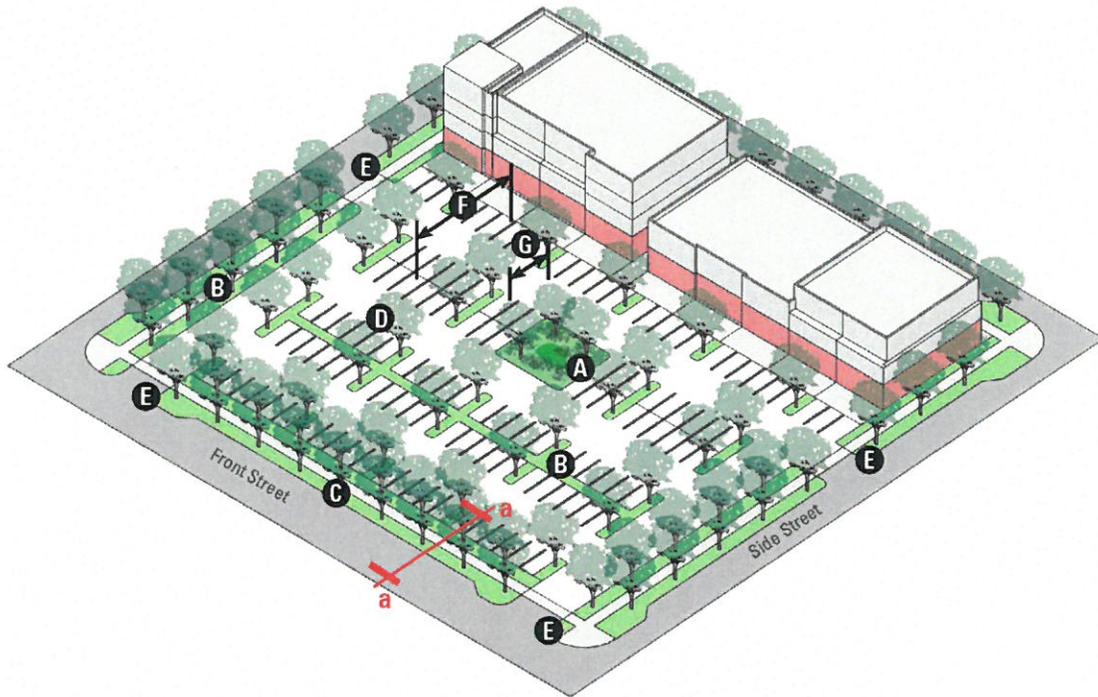
(j) Parking Lot / Structure Design.

- (1) Parking lots shall be designed to have an abundance of landscaping, including shade trees, stormwater swales, and screening from adjacent streets and sidewalks.
- (2) Parking lot dimension shall be consistent with the City of Dothan requirements.
- (3) The number of vehicular access points from streets shall be kept to a minimum to reduce the need for excessive curb-cuts.
- (4) When a parking lot is not located behind a building a screen hedge, or a low landscape wall shall be required to screen the parking lot from public rights-of-ways.
- (5) When a parking garage is proposed, it must be appropriately designed to fit into the overall context. This shall include:
  - a. Parking garages accessed from a street shall be screened using landscaping, architectural detailing, or both.
  - b. No parking garage shall exceed four (4) stories in the Downtown Core, Downtown Edge, or Mixed-Use 4 Subdistricts. Garages may be up to seven (7) stories in height.

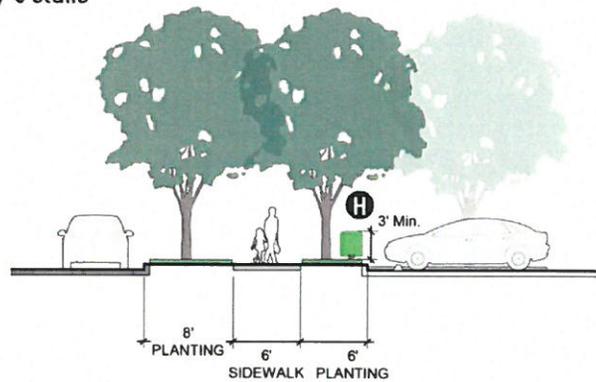
- c. Parking garages, other than individual garages associated with single-family, duplex, town home development, are not permitted in the Mixed-Use 3 or Mixed-Use 2 Subdistricts.



*Illustration of elements that meet parking lot standards*



- A** Small detention basin
- B** Bioswale
- C** Continued street trees, maximumu 30 feet apart
- D** Planter island with canopy trees every 6 stalls
- E** Parking lot entry/ exit
- F** Minimum 60 feet
- G** Minimum 24 feet
- H** Minimum 3 feet tall planting or wall



**SECTION a-a**

- (k) Parking Lot Island and Roadway Shade Trees Standards. Landscape areas shall be provided in the interior parking area to provide shade, break the expanse of pavement, provide visual relief, and minimize temperature elevation from the expanse of asphalt.
- (1) Landscape areas shall consist of planter islands, planter strips, or peninsula types. To count toward the off-street parking landscape requirements, a planter island, planter strip, or peninsula shall have a minimum area of one hundred and

twenty-six (126) square feet with a minimum width of seven (7) feet (hereafter referred to as contributing landscape areas).

- (2) Peninsulas may be designed within or adjacent to the interior parking area. The square footage of each planter island, planter strip or peninsula within the parking lot must be shown; all contributing landscape areas should be designated on the landscape plan by shading or otherwise differentiated from any non-contributing landscape area.
- (3) Foundation landscaping (landscaping immediately adjacent to building) shall not be considered interior parking area landscaping.
- (4) A list of canopy trees appropriate for shading of parking lots and streets may be found in Appendix A.
- (5) Crape Myrtle species and any other understory tree species will not be accepted toward fulfilling canopy shade requirements in parking lots and along roadways.
- (6) At intersections, tree species can change to break up the visual landscape character.
- (7) The proposed landscape plan must show how a minimum of forty percent (40%) of the parking lot will be covered with tree canopy within ten (10) years. The site landscape plan shall annotate the tree species, placement on site, and canopy dimensions. Mature canopy dimensions to be shown graphically on the site landscape plan.
- (8) Parking Lot Island and Roadway Recommended Shade Tree List. The following trees are recommended based upon documented tree characteristics including tolerance to heat and drought conditions, tree canopy for shade, lack of fruit production, seasonal color, and typical longevity. Other species may be considered at the recommendation of a licensed landscaped professional authorizing the landscape plan.
  - a. Tupelo
  - b. Chinese Elm
  - c. Chinese Pistache
  - d. Willow Oak
  - e. Purple Ash Tree
  - f. Bald Cypress
  - g. Regal Prince Oak
  - h. Green Vase Zelkova

(l) Landscape Standards.

- (1) A Landscape Plan shall be designed by a registered landscape architect or landscape designer registered in the State of Alabama.
- (2) Purpose. Vegetation and landscape are vital aspects of both the public and private realm. Urban street trees provide shade, absorb air pollution, reduce traffic speeds, create safer walking environments, increase ecological benefits, maintain crucial wildlife habitat, add value to adjacent businesses and residences, and improve the overall quality of the public realm. Providing a consistent amount of shade trees and vegetation along streets will help improve the character and health of the Overlay District.
- (3) General Landscape Requirements (Private or Public Property).

- a. Additional Trees and Planting Requirements Landscape areas should be completely covered with plants that when they reach maturity will reduce soil loss and to enhance the visual character of the area.
- b. A minimum of three inches of organic mulch shall be provided to landscape areas to limit weed growth and to enrich the soil.
- c. Shrub and ground cover spacing shall be a maximum of three feet on center or the maximum diameter of the species at  $\frac{3}{4}$  maturity, whichever is less.
- d. Tree spacing along streets should follow a consistent pattern with an emphasis on providing consistent shade.
- e. In general, the area should encompass large stately trees over small ornamental trees.
- f. Tree spacing shall allow the tree to mature without major pruning.
- g. Tree pruning shall be prohibited except for unsafe branches and to keep trees from impacting utility lines or touching buildings. Trees should be allowed to reach full height and canopy size.
- h. Space trees equal to their tree canopy width at  $\frac{3}{4}$  maturity minimum and a maximum tree spacing of forty (40) feet on center.
  - i. Plant minimum 24-inch box trees within parkways or tree wells.
  - j. Proposed plans for street trees and parking lot trees shall be accompanied by plan notations indicating specific controls to be applied for the emergence of surface roots and irrigation. Engineered tree cell products are highly encouraged. Tree cells prevent surface roots and eliminate the need for tree irrigation systems.
- k. Soil volume, tree pit dimensions and tree spacings are notably significant in the maintenance, health, and longevity of trees. It is important to allow plenty of space for trees to grow deeper. Wide spreading root systems are less likely to topple and break.
  - l. Soil volume shall be a minimum of five hundred (500) cubic feet of soil per tree.
- m. Tree Gates may be used in the Downtown Core Subdistrict and Downtown Edge Subdistrict.
- n. Trees should avoid overhead utility lines and underground utility structures. Appropriate pruning techniques should be considered in areas with overhead utilities.
- o. Understory planting should be hardy, native or climate adaptive and non-invasive plants.
- p. Rain gardens or bioswales need to be made up of water tolerant grasses and wildflowers.
- q. Planting adjacent to power lines should both consider the technical requires by the utility company for landscape that is consistent with their desired clearances while also promoting a high-quality streetscape. Continuous street trees are still required adjacent to the utility lines while also respecting setbacks as defined by the utility company.
- r. All required plant material shall be always maintained in good condition. Landscaping materials, including preserved trees that die or become diseased or damaged shall be replaced. The replacement plants shall have size equal to or greater than those being replaced. Replacement plants shall also be the same type as those being replaced unless Planning Staff determines that one or more alternate species would be more likely to thrive, in which case one of the alternate species may be used. A

landscape architect licensed by the State of Alabama, or a landscape designer licensed by the State of Alabama may recommend other species for consideration to Planning Staff that are suited to the soils and climate of Dothan, Alabama. All plant material shall be kept free of weeds, refuse and debris.

MINIMUM PLANT SIZE CHART	
Plant Material Type	Size Requirements
<b>Canopy Tree</b>	
Single Stem	2 to 2.5 inch caliper and 12 ft. height
Multi-Stem	25 gallon and 8 ft. height
Evergreen	25 gallon and 6 ft. height
<b>Understory Tree *</b>	
Single Stem	15 gallon and 6 ft. height
Multi-Stem	15 gallon and 6 ft. height
<b>Shrub</b>	
	3 gallon and 18 in. height
<b>Ground Cover</b>	
	1 gallon and 6 in. height

\* No more than 10% of the combined planting requirement may be Crepe Myrtle in any variety on a landscape plan and may not be pruned.

- A** Utility easement
- B** 8 feet Planting
- C** 6 feet Sidewalk
- D** 6 feet Planting
- E** Maximum 40 feet on enter, minimum 24 inches box
- F** New development (see sub-district design requirements for setbacks)

s. Planting Standards. Standards for transplanting should be in keeping with those established in the International Society of Arboriculture publication Tree and Shrub Planting Manual or similar publication. For plant material quality specifications, reference the American Association of Nurserymen publication American Standard for Nursery Stock (ANSI Z60, 1973). For information on tree Species, reference the American Standard for Nursery Stock (ANSI Z60, 1973). For information on tree species site requirements, reference the Manual of Woody Landscape Plants (Michael Dirr, Stipes Publishing).

1. Notes: Unless otherwise specifically indicated elsewhere in this ordinance, all plant materials shall meet the following minimum size standards: All plants should meet American Standards for Nursery

- Stock (ANSI Z60.1-1996). All minimum height requirements listed shall be planted height. Balled and burlap or field grown materials that meet caliper and height requirements and ANSI standards are acceptable.
2. Plant materials shall meet the minimum size standards as shown in the preceding chart.
  3. All site areas that are not designed for buildings and hardscape shall be planted so that when mature ninety percent (90%) of the planter area is covered with plant material.
  4. Hardscape areas shall provide shade incorporate canopy trees so that when the trees are mature, fifty percent (50%) of the hardscape areas (excluding buildings) are covered with tree canopy.
  5. All landscape areas shall be covered with at least three (3) inches of mulch at installation and new mulch shall be replaced so that soil is protected.
  6. Plant materials shall be appropriate for the climate.
  7. Tree selection shall emphasize large canopy trees rather than small ornamental trees. Small ornamental trees can be used as accents but not as the primary tree for shading.

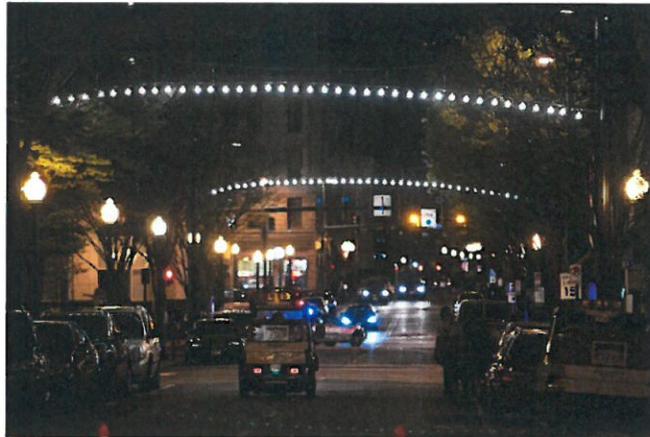
(m) Lighting Standards.

(1) Appropriate lighting is important and can serve to:

- a. Enhance visual clarity for drivers, bicyclists, and pedestrians.
- b. Aid in a pedestrian orientation when well-lit areas provide directionality.
- c. Highlight the identity of a neighborhood or community.
- d. Enable areas in the public realm (plazas, parks to be more conducive to pedestrian/ merchant activities).



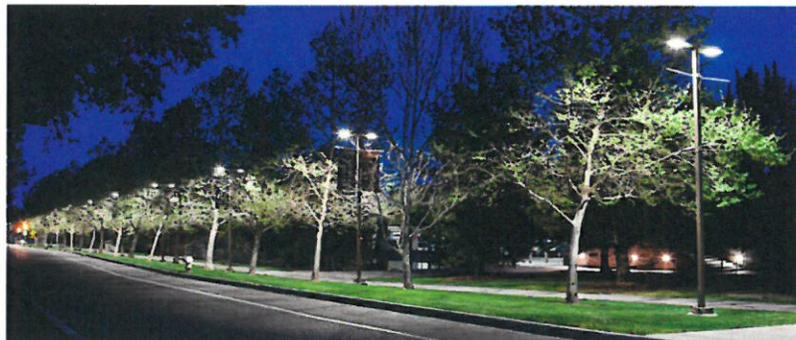
*Pedestrian scaled lighting*



*Accent lighting over festival street*



*Lighting fixture to complement historic district*



*Lighting fixtures to service both pedestrians and vehicles*

- (2) A hierarchy of lighting types is encouraged to assist drivers, bicyclists, and pedestrians.
- (3) Lighting shall be scaled to the context of the area.
- (4) Reduce glare by utilizing appropriate shielding.
- (5) Light temperature shall be warm (in yellow tones) rather than bright whites.
- (6) Lighting that blinks or oscillates is prohibited.

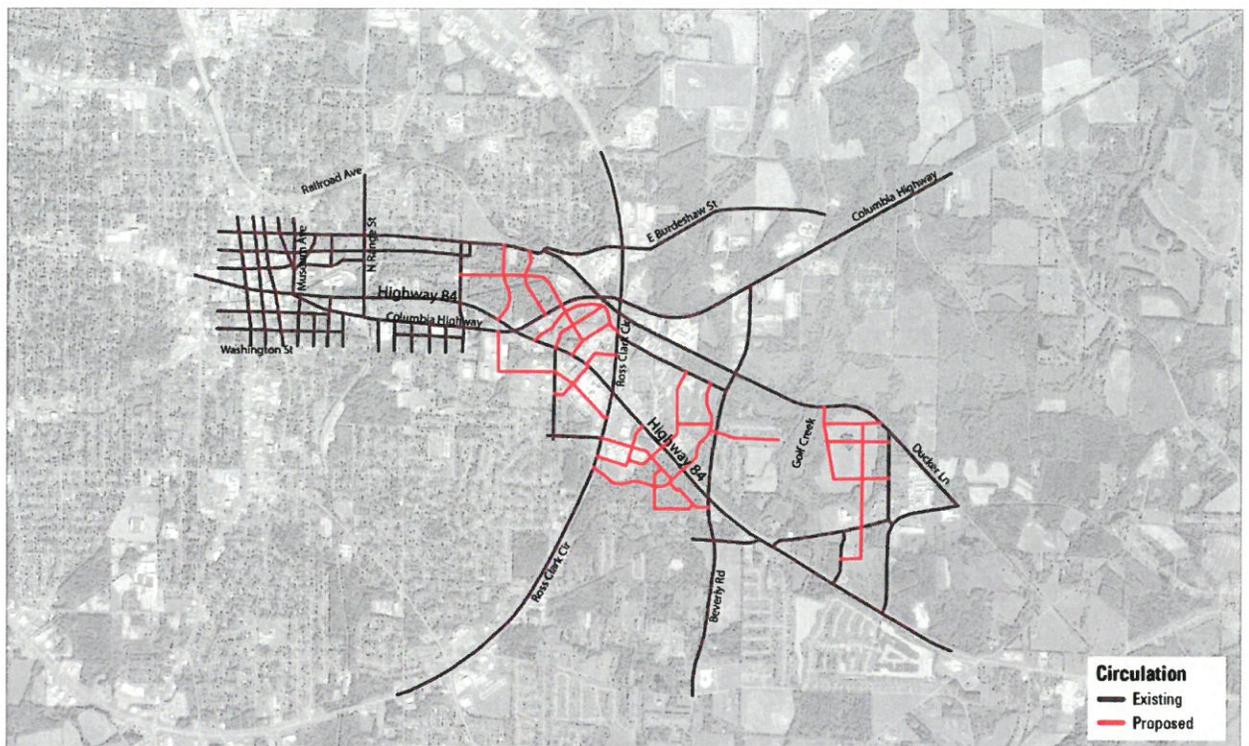
(n) Paving Standards.

- (1) Paving consistency is a priority to enhance the design continuity of individual developments.
  - a. Paving shall be neutral colors (earth tones or grays).
- (2) Simple, well installed concrete sidewalks is preferred over poorly installed enhanced pavement.
- (3) Stamped concrete is discouraged.
- (4) Concrete shall have equally spaced expansion joints to reduce cracking and to prolong maintenance.
- (5) The use of permeable paving in select areas is encouraged to slow stormwater runoff and to provide accents.
  - a. Permeable pavers shall be neutral colors (earth tones or grays).

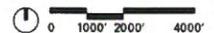
(o) Circulation Plan.

- (1) The purpose of the Circulation Plan is to demonstrate the general location of major roadways within the Overlay District to ensure that adjacent developments maintain a minimum acceptable level of connectivity.
- (2) Map 3 outlines the desired circulation within the Overlay. The ultimate locations of roadways will be determined as part of the application review by Development Review Committee (DRC).

**Map 3 – Circulation**



**HIGHWAY 84 OVERLAY DISTRICT  
CIRCULATION MAP**

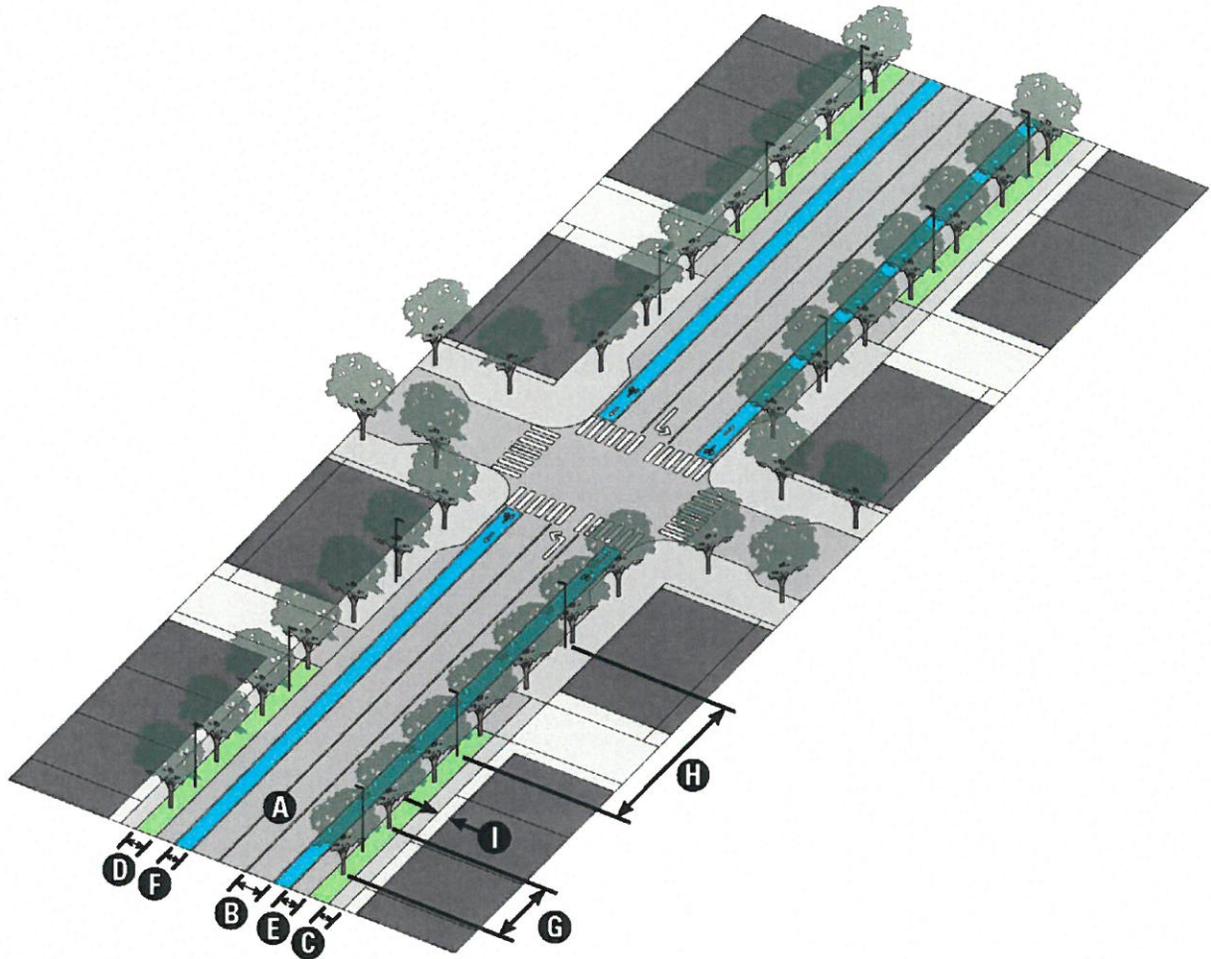


- (3) The overall street network shall be consistent with the intent of the Circulation Plan of this section.
  - (4) Adjustments to the Circulation Plan may be required due to constraints, however the applicant must show how modifications to the circulation plan will result in maintaining the connectivity of the existing circulation plan or enhance it.
  - (5) Circulation between adjacent properties shall be created to the greatest extent possible. This may require coordination between property owners to determine the final location of street or pathway connections.
  - (6) All property boundaries must be addressed as part of overall connectivity.
  - (7) Amendments to the Circulation Plan shall be reviewed and approved by Staff.
- (p) Requirements for Major Projects. Any project that is equal to or greater than four (4) acres in size (as a single parcel, or as a compilation of multiple parcels) is considered a Major Project within the Overlay area. In addition to the General Requirements that apply to all properties in the Overlay, the following additional requirements shall apply.
- (1) Coordination between adjacent properties. Major Projects are required to provide a coordinated, interconnected street network, providing connections for both pedestrians and vehicles.
    - a. The applicant must show how the street network will connect with adjoining properties. Streets should be designed and built so future connection can occur when development on the adjoining parcels occur.

- b. When considering connections, all property boundaries shall be considered and addressed.
  - c. Private and public streets shall comply with the intent of the Highway 84 East Vision Plan and with the Streets section in this Chapter.
  - d. Every building shall be accessible to service and emergency vehicles.
- (2) Subdistrict Plan.
- a. The Overlay Subdistrict map shall be updated and refined to reflect proposed circulation and dedicated accessible open space and parks. The applicant shall prepare a revised Subdistrict map for submittal with the application for major projects.
- (3) Open Space Plan.
- a. All open space not assigned to public use or to private occupancy shall be assigned to the common use with such use ensured in perpetuity. Assignment and development of such open spaces shall provide for the common enjoyment of all.
- (4) Park Requirements.
- a. Parks shall have a minimum acreage of ten percent (10%) of the total development area for Major Projects over four (4) acres in size.
  - b. The minimum width to be counted as a park shall be sixty (60) feet.
  - c. Park must be accessible from a public right-of-way on at least two (2) sides unless site constraints prevent this.
- (q) Street Standards.
- (1) General Requirements. Great streets are more than a place for automobiles. Great cities have streets that function as the primary public realm for people to gather, to walk, to bike, to eat a meal, to be outside, as well as a place to drive. Within the Overlay area streets shall be modified, block by block, with a combination of public and private investment.
- a. The Highway 84 East Vision shows how street design transforms through the addition of street trees, continuous walkways, and trails, improving pedestrian crossings and safety and moderating speeds.
  - b. The Circulation Plan provides conceptual guidance for the new street network to be designed and built with new development.
  - c. The new street network shall be a connected grid with no dead ends or cul-de-sacs.
  - d. The new street network shall function as multi-modal corridors with Complete Street elements.
  - e. All public and private streets shall comply with this section.
- (2) Open and Continuous.
- a. Streets shall be always open to the public. Gating of streets and alleys is not permitted.
- (3) Service Areas.
- a. Areas for service, deliveries, trash pick-up, and the like, should be located along rear alleys and parking lots rather than from public streets or in the front of buildings.
- (4) Collector Streets.
- a. A network of new collector streets shall be built consistent with the Highway 84 East Vision and with the Circulation Plan in this Chapter.
  - b. Street Design requirements are outlined in Table 7 and in Figure 9.

<b>Table 7 Collector Street Standards</b>		
<b>Street Design Element</b>	<b>Dimension / Design Requirements</b>	<b>Legend on Image</b>
Number of Lanes	Varies based on traffic volumes	A
Lane Width	11 feet	B
Sidewalk Width	6 feet min.	C
Landscape Zone	8 feet min.	D
Parking	8 feet	E
Bicycle Lane	6 feet min.	F
Street Tree Spacing	30 feet on center max.	G
Street Lighting	60 feet on center max.	H
Building Footprint	Setback per subdistrict development standards	I

**Figure 9: Collector Street Landscape and Street Standards**

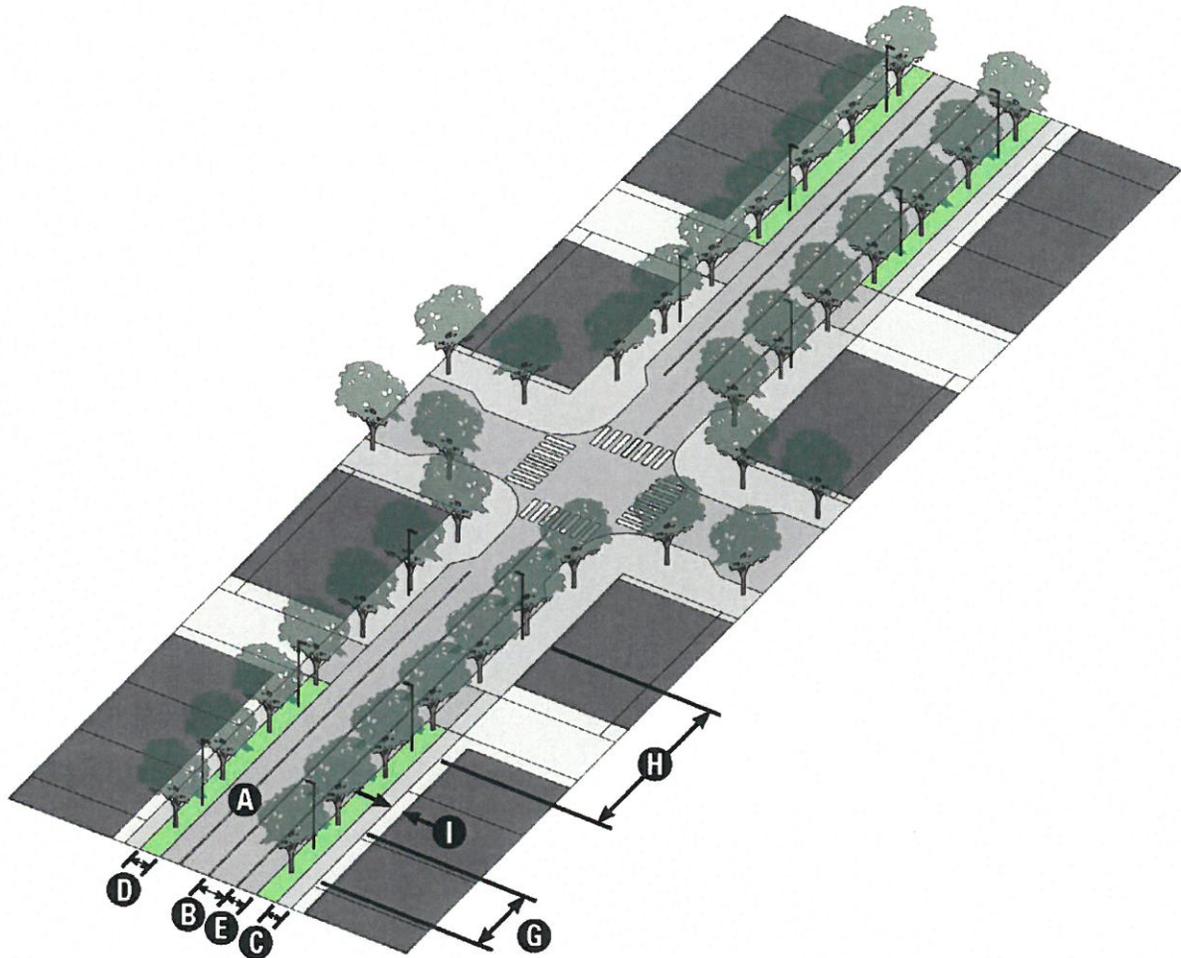


- A** Number of Lanes: Varies based on traffic volumes
- B** Lane Width: 11 feet
- C** Sidewalk Width: 6 feet min.
- D** Landscape Zone: 8 feet min.
- E** Parking: 8 feet
- F** Bicycle Lane: 6 feet min.
- G** Street Tree Spacing: 30 feet on center max.
- H** Street Lighting: 60 feet on center max.
- I** Building Footprint: Setback per sub-district development standards

- (5) Neighborhood Streets with Street Parking.
  - a. Neighborhood streets with on-street parking will make up most new streets through the Overlay project area. Providing on-street parallel parking will allow for reduced parking lot sizes and reduced parking ratios for new development.
  - b. Street Design requirements are outlined in Table 8 and in Figure 10.

<b>Table 8</b>		
<b>Neighborhood Streets with Street Parking Standards</b>		
<b>Street Design Element</b>	<b>Dimension / Design Requirements</b>	<b>Legend on Image</b>
Number of Lanes	2	A
Lane Width	11 feet	B
Sidewalk Width	5 feet min.	C
Landscape Zone	6 feet min.	D
Parking	8 feet	E
Bicycle Lane	6 feet min. (if proposed)	F
Street Tree Spacing	30 feet on center max.	G
Street Lighting	60 feet on center max.	H
Building Footprint	Setback per subdistrict development standards	I

**Figure 10: Neighborhood Street with Street Parking Landscape and Street Standard**



- A** Number of Lanes: 2
- B** Lane Width: 11 feet
- C** Sidewalk Width: 5 feet min.
- D** Landscape Zone: 6 feet min.
- E** Parking: 8 feet

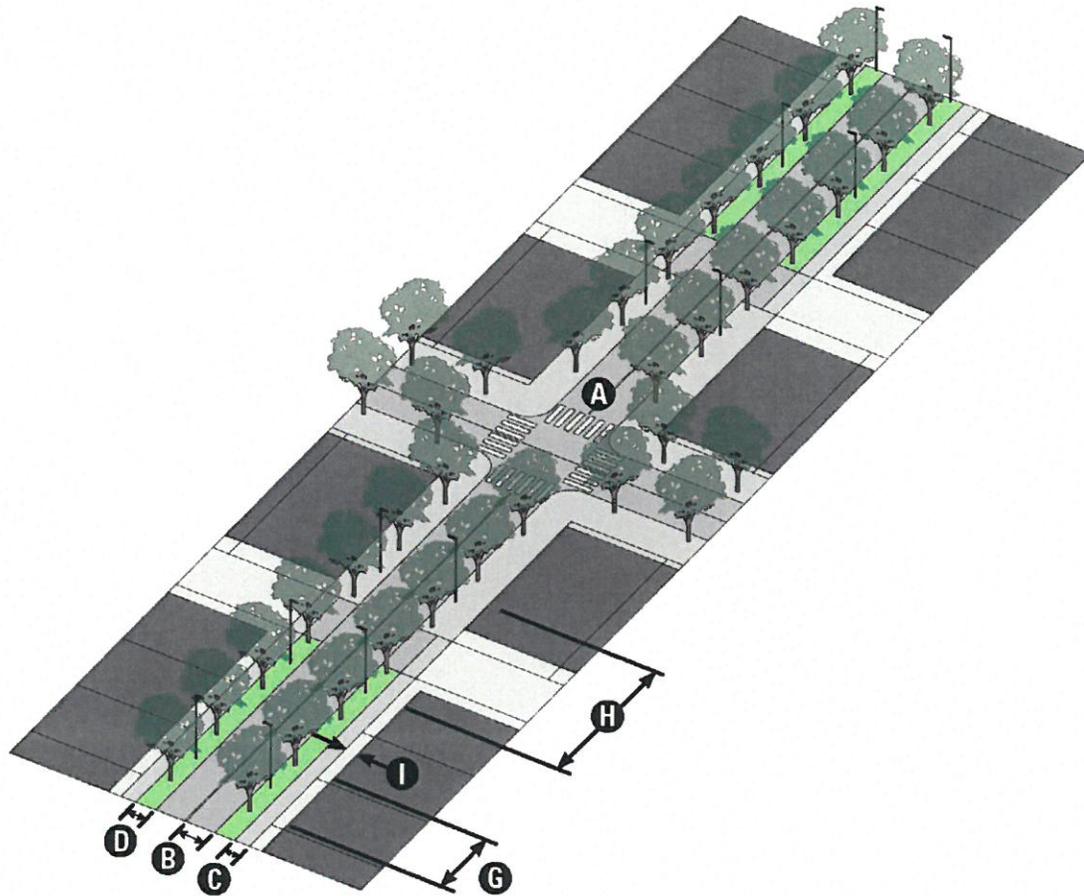
- F** Bicycle Lane: 6 feet min. (if proposed)
- G** Street Tree Spacing: 30 feet on center max.
- H** Street Lighting: 60 feet on center max.
- I** Building Footprint: Setback per sub-district development standards

(6) Neighborhood Streets without Street Parking.

- a. Neighborhood streets without on-street parking may be necessary in specific conditions to reduce the right-of-way width, or when street parking is not necessary because there is sufficient parking in lots or within a walkable distance to destinations. This determination shall be made by Staff.
- b. Street Design requirements are outlined in Table 9 and in Figure 11.

<b>Table 9</b>		
<b>Neighborhood Streets without Street Parking Standards</b>		
<b>Street Design Element</b>	<b>Dimension / Design Requirements</b>	<b>Legend on Image</b>
Number of Lanes	2 lanes	A
Lane Width	11 feet	B
Sidewalk Width	5 feet min.	C
Landscape Zone	6 feet min.	D
Parking	0 feet	E
Bicycle Lane	6 feet min. (if proposed)	F
Street Tree Spacing	35 feet on center max.	G
Street Lighting	60 feet on center max.	H
Building Footprint	Setback per subdistrict development standards	I

**Figure 11: Neighborhood Street without Street Parking Landscape and Street Standard**



- A** Number of Lanes: 2
- B** Lane Width: 11 feet
- C** Sidewalk Width: 5 feet min.
- D** Landscape Zone: 6 feet min.
- E** Parking: 0 feet

- F** Bicycle Lane: 6 feet min. (if proposed)
- G** Street Tree Spacing: 35 feet on center max.
- H** Street Lighting: 60 feet on center max.
- I** Building Footprint: Setback per sub-district development standards

- (7) Intersection Design. Should include roundabouts where appropriate.
  - a. Corner Radius: Twenty (20) foot maximum radius to promote slower traffic turning movements resulting in a safer pedestrian environment.
  - b. Bulb Outs: Where feasible curb extensions should be built, extensions should be a minimum of six (6) feet.
  - c. Crosswalks: High visibility crosswalks at all intersections are required. Crosswalks should be a minimum of ten (10) feet in width.
  
- (r) Approval Process.
  - (1) Pre-Application Meeting. Applicants are encouraged, but not required, to schedule an initial planning meeting with the Planning Staff to discuss the scope and intent of the project and to ensure understanding of the Overlay requirements and applicability.
  - (2) Development Plan Approval. An approval, pursuant to Article V of the Dothan Zoning Code is required for all development in the Highway 84 East Overlay. An application shall follow the steps and requirements outlined in Article V, except that the following additional requirements shall apply.
    - a. A written description of the proposal, including how the proposal meets the requirements of Highway 84 East Overlay and the Highway 84 East Vision Plan. This description must address:
      1. Circulation for pedestrians, bicycles, and automobiles;
      2. Open space and parks;
      3. Street design; and
      4. Land use.
    - b. A Landscape Plan, shall be designed by a registered landscape architect or landscape designer registered in the State of Alabama, shall be included as part of the. It shall identify all the proposed plant material, location, and grading proposed. The plan must include required notations regarding tree and plant species, tree canopy size at maturity, tree root controls, irrigation plan, and long-term maintenance plan.
    - c. Mapping, renderings, or other visuals that clearly illustrate how the proposal complies with this Chapter related to Parking, Streets, Circulation, Landscaping, and Signage.
    - d. All applications shall include a written and graphic description of how existing and proposed streets, sidewalks, and bike lanes, comply with the Overlay's Circulation Plan and will result in a coordinated, interconnected street network with coordinated blocks among different parcels with different owners.
    - e. Architectural Plans, designed by a registered designer, shall be included. At a minimum, building elevations showing the following must be included:
      1. Materials palette keyed to elevation;
      2. Color palette keyed to elevation; and
      3. Written description of how the architecture fits with the project context and the Overlay's design requirements.
    - f. Conceptual drainage, water, sewerage, street plans and specifications of proposed improvements prepared in sufficient detail to indicate the proposal will meet City of Dothan requirements.

- (3) Deviations from the development standards within this Overlay will require the approval of the Planning Commission.
- (4) Modifications to an approved project within the Overlay District.
  - a. Minor. Minor modifications of the approved plan during construction may be permitted upon consultation with the administrative official who, in turn shall consult with the Development Review Committee (DRC) as appropriate. Minor modifications shall not include the following: the removal of primary roads, reduction of open space, alteration of pedestrian facilities, or the reallocation of land uses.
  - b. Major. If a substantial amendment to an approved plan is submitted, the requirements will be the same as if submitting the entire project for approval and shall comply with the City of Dothan Code of Ordinances in place at the time of the amended application.

**Section 3.** That the City Clerk of the City of Dothan is hereby ordered and directed to cause this ordinance to be published.

**Section 4.** That this ordinance and the rules, regulations, provisions, requirements, orders, and matters established and adopted hereby shall take effect and be in full force and effect immediately from and after the date of its final passage and adoption.

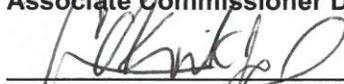
PASSED, ADOPTED, AND APPROVED ON August 16, 2022.



Mayor



Associate Commissioner District 1



Associate Commissioner District 2



Associate Commissioner District 3



Associate Commissioner District 4



Associate Commissioner District 5



Associate Commissioner District 6

**BOARD OF CITY COMMISSIONERS**

ATTEST:



City Clerk

I hereby certify that a synopsis of the above Ordinance was published in THE DOTHAN EAGLE, a newspaper of general circulation in the City of Dothan, Alabama, on August 18, 2022.



Wendy Shiver  
City Clerk